



TOLL BRIDGE PROGRAM OVERSIGHT COMMITTEE

MEETING MATERIALS

June 27, 2007

CALTRANS

BAY AREA TOLL AUTHORITY

CALIFORNIA TRANSPORTATION COMMISSION





Letter of Transmittal

TO: Toll Bridge Program Oversight Committee
(TBPOC)

DATE: June 21, 2007

FR: Program Management Team (PMT)

RE: TBPOC Conference Call Materials Packet – June 27, 2007

Attached is the TBPOC Conference Call Materials Packet for the June 27th conference call. The packet includes memoranda and supporting documents that will be presented at the teleconference. A Table of Contents is provided following the Agenda to help locate specific topics.

**TBPOC CONFERENCE CALL
June 27, 2007, 4:30 PM - 5:00 PM**

Topic	Presenter	Time	Desired Outcome
1. TBPOC Chair Policy*	S. Maller, CTC	2 min	Approval
2. San Francisco-Oakland Bay Bridge			
a. Labor Day Weekend Closure for Detour West Tie-In Work/YBI Viaduct Replacement*	T. Anziano, CT	3 min	Approval
b. Yerba Buena Island	T. Anziano, CT	5 min	Approval
1) CCO Implementation Strategy for SSD*			
2) CCO's**			
c. West Approach CCO*	T. Anziano, CT	5 min	Approval
d. Skyway CCO*	T. Anziano, CT	5 min	Approval
e. Schedule and Budget Revisions for Stormwater Treatment Measures*	T. Anziano, CT	5 min	Approval
3. New Benicia-Martinez Bridge			
a. Construction of Modifications to Existing Bridge*	T. Anziano, CT / P. Lee, BATA	5 min	Approval
4. Other Business	W. Kempton, CT		n/a
Next Meeting: Tuesday, August 2, 2007, 1:00 p.m. – 4:00 p.m., Casa de la Vista, Treasure Island			

* Attachments

** Final Documents still in process; to be provided as soon as available.

*** Stand alone document included in the binder.



Table of Contents

TBPOC Conference Call June 27, 2007

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1	1	TBPOC Chair Policy*
2	2	SAN FRANCISCO-OAKLAND BAY BRIDGE UPDATES
		<ul style="list-style-type: none"> a. Labor Day Weekend Closure for Detour West Tie-In Work/YBI Viaduct Replacement* b. Yerba Buena Island <ul style="list-style-type: none"> 1) CCO Implementation Strategy for SSD* 2) CCO's** c. West Approach CCO* d. Skyway CCO* e. Schedule and Budget Revisions for Stormwater Treatment Measures*
3	3	NEW BENICIA-MARTINEZ BRIDGE <ul style="list-style-type: none"> a. Construction of Modifications to Existing Bridge*
4	4	OTHER BUSINESS (No attachments)

* Attachments

** Final Documents still in process; to be provided as soon as available

*** Stand alone document included in the binder

Item 1: TBPOC Chair Policy

Memorandum

TO: Toll Bridge Program Oversight Committee **DATE:** June 21, 2007

FR: Stephen Maller, CTC Deputy Director

RE: Agenda No. - 1

Item - TBPOC Chair Policy

Cost:

N/A

Recommendation:

N/A

Discussion:

Background

On November 21, 2005, the TBPOC approved the "Agreement on Committee Procedures for the Toll Bridge Program Oversight Committee." This Agreement is non-binding. In response to TBPOC request, three options have been developed regarding the term of the Chairperson for the TBPOC. The following page outlines options for consideration.

A copy of the currently approved Agreement is attached.

Memorandum

The Agreement states in Section II, Part B (Chairperson):

“The Committee will select a Chairperson. The Chairperson position will rotate between the members affiliated with the Establishing Agencies for every two years. The Chairperson shall preside over the meetings of the Committee and shall perform all other duties incident to the position or as assigned to him or her by the Committee.”

Options for Consideration for TBPOC Chairperson Role:

Status Quo: BATA TBPOC Member Steve Heminger named Chairperson until July 2009.

Rotate Between the Department, BATA, and CTC: The role of Chairperson will be rotated every 2 years among the Establishing Agencies (Department, BATA) and the CTC.

Will Kempton, to be appointed as Chairperson for the duration of his tenure as the Director of the Department.

Attachment:

Agreement on Committee Procedures for the Toll Bridge Program Oversight Committee

**AGREEMENT ON COMMITTEE PROCEDURES
FOR THE TOLL BRIDGE PROGRAM OVERSIGHT COMMITTEE**

This Agreement is entered into and effective this 9th day of November, 2005, by and among the Director of the California Department of Transportation, (the "Department"), the Executive Director of the Bay Area Toll Authority (the "Authority") and the Executive Director of the California Transportation Commission (the "Commission"), for the purpose of outlining the roles and areas of responsibility set out in Chapter 71, Statutes of 2005, related to the duties and responsibilities of the Toll Bridge Program Oversight Committee. The Department, Authority and Commission are collectively referred to as the "Agencies."

RECITALS

WHEREAS, the California Streets and Highways Code (SHC) Section 330952.1 requires the Department and Authority hereinafter referred to collectively as the "Establishing Agencies," to establish a Toll Bridge Program Oversight Committee, hereinafter referred to as the "Committee," consisting of the Director of the Department, the Executive Director of the Authority and the Executive Director of the Commission, hereinafter collectively referred to as the "Committee Members;" and

WHEREAS, the Committee Members desire to establish an agreement outlining their roles and responsibilities in carrying out the work of the Committee;

NOW, THEREFORE, the Committee Members hereto agree as follows:

I. TERM.

The term of this Agreement shall commence when fully executed, and unless amended earlier, shall terminate when the Bridge Projects have been accepted by the Department, the Bridge Projects contractor claims have been resolved through settlement or public works arbitration and environmental mitigation has been concluded.

II. TOLL BRIDGE PROGRAM OVERSIGHT COMMITTEE MANAGEMENT AND ORGANIZATION

A. Committee Members Qualifications

In the event a Committee Member, for whatever reason, no longer serves in his or her Director's position with his or her respective Agency, the Committee

Member shall be replaced by his or her successor or acting successor, as determined by that Committee Member's Agency.

B. Chairperson

The Committee will select a Chairperson. The Chairperson position will rotate between the members affiliated with the Establishing Agencies every two years. The Chairperson shall preside over the meetings of the Committee and shall perform all other duties incident to the position or as assigned to him or her by the Committee.

C. Decision-making

The Committee will endeavor to make decisions on a consensus basis. When a vote by the Committee is necessary, a majority vote of Committee Members is required to approve an item. Every act or decision made by the majority vote of the Committee Members is an act of the Committee. A quorum of the Committee is two. A meeting at which all the Committee Members are initially present may continue to make decisions and transact business notwithstanding the withdrawal of one of its members.

D. Responsibilities

The Committee will:

1. Provide oversight and financial direction for the Bridge Projects.
2. Review and approve project reporting of the Bridge Projects status, program costs and schedules and provide reports to the Authority on a monthly basis.
3. Approve all contracts for project oversight and control for the Bridge Projects.
4. Review and recommend for approval contract specifications and bid documents for the Bridge Projects.
5. Resolve project budget issues and review and recommend budget and fund allocation adjustments.
6. Evaluate Bridge Project changes and review and approve significant change orders and claims over one million dollars (\$1,000,000).
7. Develop and regularly update cost estimates, risk assessment, and cash flow requirements for all phases of the Bridge Projects.
8. Review staffing structures and levels for the Bridge Projects.
9. Review and approve consultant and contractor services related to the oversight duties of the Committee for the Bridge Projects.

10. Report to the Transportation and Fiscal committees of both houses of the Legislature and the Commission on a quarterly basis, as specified in SHC Section 30952.2(b).
11. Assume such other responsibilities as may be assigned to it by the Agencies or as a result of subsequent legislative amendments.

E. Meetings

Regular meetings of the Committee shall be held monthly or as otherwise determined by the Committee. Special meetings of the Committee can be held for any purpose, by any method, including the use of conference telephone, electronic video screen communication or other electronic communications equipment, so long as all members participating in such meeting can concurrently communicate with the other members. Meetings may be called at any time by the Chairperson or any other of the Committee Members. Notice of all meetings shall be given at least two business days prior to the meeting. Notice shall include an agenda of items on which the Committee will take action. Any member of the Committee has the right to request that action on a particular item be deferred to allow for further review of the proposed item. Upon such a request, action on that item will be deferred for the time period requested by that member, up to a maximum of seven days. Each member of the Committee has the right to place a matter on the Committee's agenda for consideration.

F. Actions Without a Meeting

Any action required or permitted to be taken by the Committee may be taken without a meeting by way of written memorandum if all members of the Committee, individually or collectively, consent in writing to that action. The written consent or consents shall be filed with the minutes of the Committee. Action by written consent shall have the same force and effect as a vote of the Committee Members taken during a meeting.

G. Records, Minutes of Meetings, and Inspection Rights

The Committee shall keep all records, documents and minutes of meetings at the principal executive offices of the Department. In the event a request for records or documents generated for or by the Committee is received by a member of the Committee, the member shall, within 24 hours, notify all other Committee members of the request.

H. Project Management Team

The Committee hereby establishes a Project Management Team (PMT) that shall assist the Committee in the performance of its duties. The PMT shall consist of one staff member selected by each member of the Committee. The members of the PMT shall review matters that are to be brought before the Committee.

At the request of the Committee, the PMT may perform the following:

- Prepare agendas for the Committee's meetings.
- Assist the Committee in the performance of its duties by providing regular reports to the Committee on Bridge Project status, scope and issues involving budgeting, expenditures, staffing and contractor services.
- Assist the Committee in the review of contract specifications and bid documents, and other documents.
- Assist the Committee in the review of project status and schedules and to anticipate, identify, evaluate, and report to the Committee concerning any project issues as they arise.
- Assist the Committee in the development of cost estimates, risk assessments, and cash flow requirements.
- Review proposed contract change orders for Committee consideration and approval.
- Review claims for Committee consideration and approval.
- Assist the Committee in reviewing staffing levels and structures.
- Prepare other project related reports for Committee review.
- Perform such other assignments as appropriate.
- In carrying out the above tasks, seek assistance whenever appropriate from consultants retained by any of the Agencies doing work related to the Bridge Projects.

The PMT shall keep Committee Members informed as to its work, and will promptly provide any information in its possession which may be requested by a Committee Member.

I. Advance Notice of Significant Issues

Each Committee Member will provide to the other Committee Members and to the PMT advance notice of significant change orders and claims and other potential action items which are likely to be brought before the Committee by the Agency with whom that Member is associated in order to provide the Committee Members an adequate opportunity for review and preparation.

III. GENERAL

A. Integration Clause

This Agreement constitutes the complete and entire understanding among the Committee Members.

B. Amendments

This Agreement may be amended in writing from time to time upon agreement of the Committee Members.

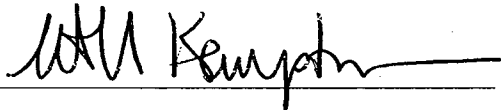
C. Counter Parts

This Agreement may be executed in counterparts, each one of which will be an original or the equivalent thereof.

D. Miscellaneous

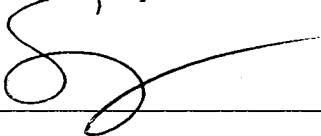
This Agreement is intended solely as a guide to the obligations, intentions and policies of the Committee Members. It does not constitute an authorization for funding a project nor does it constitute a legally binding agreement amongst the Agencies.

IN WITNESS WHEREOF, the Committee Members hereto have agreed to this Agreement on the date opposite their respective names.




Date: 11/21/05

Will Kempton
Director, California Department of Transportation



Date: 11/21/05

Steve Heminger
Executive Director, Bay Area Toll Authority



Date: 11/21/05

Diane C. Eidam
Executive Director, California Transportation Commission

Item 2: San Francisco-Oakland Bay Bridge
a. Labor Day Weekend Closure for
Detour West Tie-In Work/YBI
Viaduct Replacement

TO: Toll Bridge Program Oversight Committee (TBPOC) DATE: June 21, 2007

FR: Tony Anziano, Toll Bridge Program Manager, Caltrans

RE: Agenda No. - 2a

San Francisco-Oakland Bay Bridge
Item- Labor Day Weekend Closure for Detour West Tie-In Work/YBI
Viaduct Replacement

Recommendation:

Approve final communications message for the bridge closure (3-day versus 4-day) required for the Detour West Tie-In/ Yerba Buena Island Viaduct Replacement work that will be occurring Labor Day weekend of 2007.

If there will not be construction conflicts, approve utilizing the Labor Day closure to re-stripe the entire toll plaza area from the metering lights to the Maze.

Discussion:

Background

During the months of February, April, and May, the TBPOC was presented information and recommendations regarding the demolition and replacement of the Yerba Buena Island Viaduct over Labor Day Weekend 2007. History of TBPOC actions include:

- Approving a strategy for retrofit of the Yerba Buena Island (YBI) Viaduct and construction of the first phase of the West Tie-In (WTI) for the Detour that will require a complete closure of the San Francisco-Oakland Bay Bridge (SFOBB) over Labor Day weekend of 2007;
- Approving a communications and outreach plan for Labor Day bridge closure, except deferring final decision as to whether the plan would definitely refer to a three-day or four-day closure, with the difference being closing the bridge late Thursday night (August 30) versus closing the bridge Friday night (August 31)

- Deferring decision on final bridge closure communications message (3-day versus 4-day) until June TBPOC meeting.

Analysis

Detailed plans and schedules from the Contractor (CC Myers) have been provided and reviewed by the Department, BATA, and CTC staff. Staff from the Department, BATA, and CTC concur that 3 days is a sufficient amount of time necessary to complete the work as planned. The PMT and staff will continue to further refine and examine hours of float.

To fully implement the BATA FasTrak® Strategic Plan, BATA proposes to utilize the Labor Day Bridge Closure as an opportunity to re-stripe the entire toll plaza area from the metering lights to the Maze. Currently, CC Meyer's demolition plans do not utilize the toll plaza area that is proposed to be re-striped. BATA staff continues to work with Caltrans Construction staff to ensure that there will be no construction impacts between the two activities.

Attachment(s):

- 1) Memo from May 1, 2007 TBPOC Meeting
- 2) Silverado Demolition Plan

Memorandum

TO: Toll Bridge Program Oversight Committee **DATE:** April 26, 2007

FR: Tony Anziano, Toll Bridge Program Manager, Caltrans

RE: Agenda No. - 5a, 1

Item- Yerba Buena Island
Labor Day Weekend Closure for Detour West Tie-In Work/ YBI
Viaduct Replacement

Recommendation:

Approve final communications message for the bridge closure required for the South-South Detour West Tie In/Yerba Buena Island Viaduct work that will be occurring Labor Day weekend of 2007.

Discussion:

Background

In February 2007, the Toll Bridge Program Oversight Committee (TBPOC) approved a strategy for retrofit of the Yerba Buena Island (YBI) Viaduct and construction of the first phase of the West Tie-In (WTI) for the South-South Detour (SSD) that will require a complete closure of the San Francisco-Oakland Bay Bridge (SFOBB) over Labor Day weekend of 2007.

In April 2007, the TBPOC approved a communications and outreach plan for the Labor Day bridge closure, except that the TBPOC deferred final decision as to whether the plan would definitely refer to a three day or a four day closure, with the difference being closing the bridge late Thursday night (August 30) versus closing the bridge Friday night (August 31).

Memorandum

The YBI Viaduct/WTI phase 1 work involves demolition of approximately 3,500 cubic yards of material and roll-in of a 350 foot long by 95 foot wide concrete superstructure (the top deck of the viaduct) that weighs 6,000 tons. The initial schedule developed in by the SSD contractor, CC Myers, indicated that 77 hours would be required for demolition and reconstruction of the YBI Viaduct including WTI phase 1 work. The first revision to this schedule reduced the required time to 72 hours and included 5 hours of float (time not planned for use but available if necessary) and assumed a start time of 11:59 pm Friday night, August 31.

CC Myers has developed a solid team to perform the work. Demolition services will be provided by Silverado Contractors, Inc. a demolition expert with a history of successful partnership in projects with CC Myers. The roll-in of the new YBI Viaduct structure will be performed by Mammoet, a contractor specializing in heavy lifts and transport of heavy structures. Mammoet is the world leader in heavy moves and lifting, and holds several world records for such lifts and moves. Mammoet has moved bridge structures before, including similar structures weighing 6,000 tons and 9,800 tons (this move included the superstructure and abutments).

CC Myers has refined their schedule and currently has expanded available float to 10 hours. The approximate breakdown of work is 46 hours for demolition, 12 hours for roll in, and 9 hours for paving and striping (46 demo + 12 roll in + 9 paving/striping + 10 float = 77 hours). This continues to assume a start time of 11:59 pm Friday, August 31.

Analysis

If the bridge closure time is pulled back to 7:00 pm on Friday, August 31, the bridge will be clear no later than 9:00 pm and work can begin. This will provide an additional 3 hours of float. CC Myers is in the process of securing a specialized crane system that will add efficiency to the demolition operation that will add another 5 hours of float. This will bring total float to 18 hours, 4 hours short of a complete day. Further refinement of planned activities may create

Memorandum

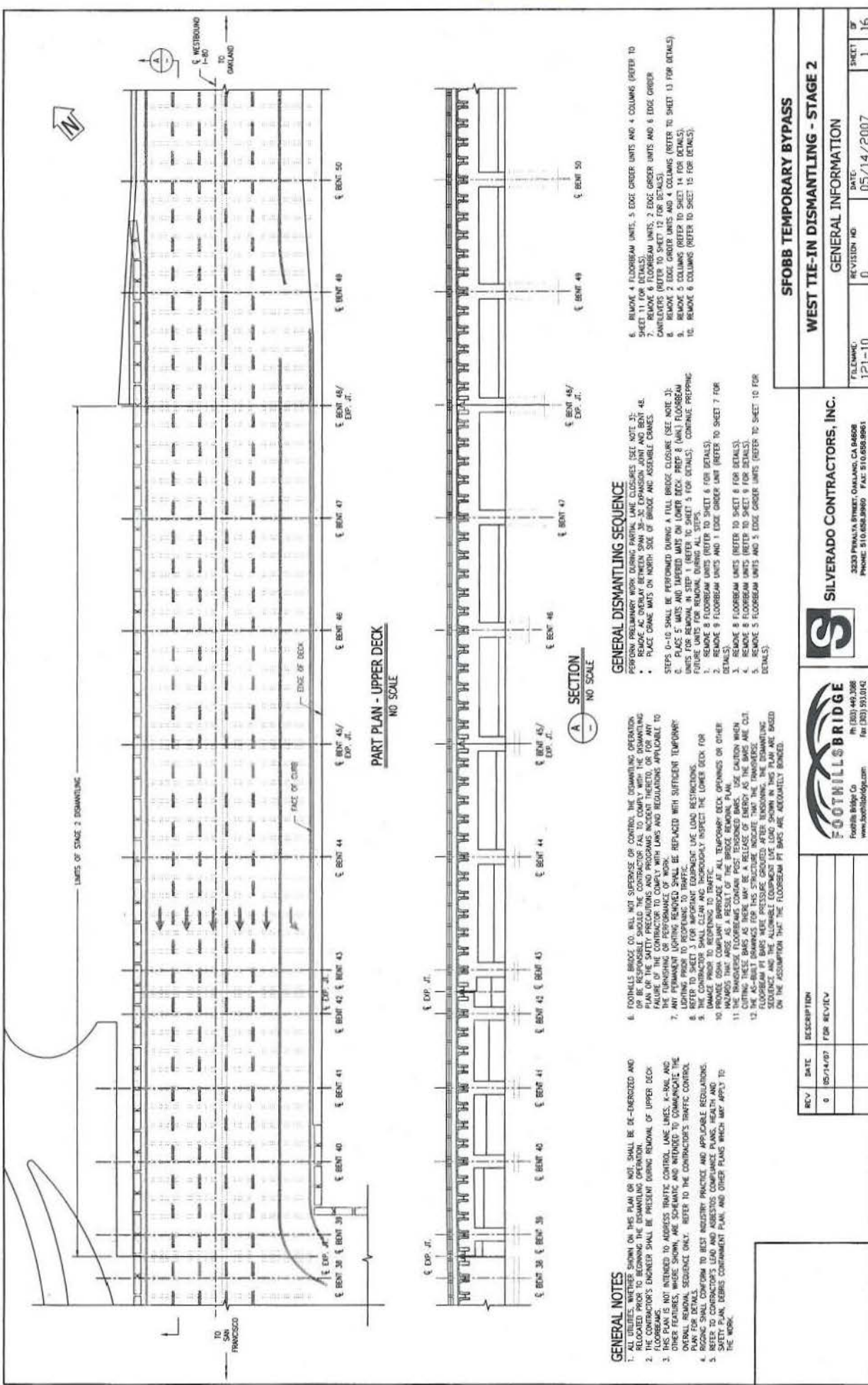
opportunities for additional float. For example, early closure of the YBI eastbound on ramp to the SFOBB will allow Mammoet to place the most difficult portion of the concrete foundation for its skid system before demolition work begins. This will require coordination with the Treasure Island Development Authority and the United States Coast Guard.

A three day time frame is also validated by comparable projects. The West Approach Labor Day 2006 demolition required 77 hours, but certain areas of work had 18 hours less due to work stoppages required for bus use of the SFOBB. The demolition involved 10,000 cubic yards of concrete. 8,000 cubic yards of concrete were recently removed on the West Approach project over a 30 hour period in a confined area. There are however, logistical difficulties on YBI not present in the West Approach demolition work. This would indicate that a period of 46 hours, the time required for demolition under CC Myers current schedule, is a very reasonable assumption for the YBI Viaduct/WTI demolition work.

A similar roll in operation was completed for a freeway overcrossing in Bellevue Washington. The structure was 328 feet long by 61 feet wide weighing 2,200 tons. Roll in was completed in 10 hours. Mammoet's prior roll ins of the 6,000 and 9,800 ton bridge structures were completed in under 10 hours. This indicates that the planned 12 hours for roll in is reasonable.

Attachment(s):

- 1) Silverado Contractors, Inc brochure
- 2) Mammoet brochure
- 3) 2 Informational Handouts for Bellevue Washington Roll In



GENERAL NOTES

1. ALL WORK SHALL BE DONE ON THE PLAN OR NOT SHOWN BE DE-ENERGIZED AND ISOLATED PRIOR TO BEGINNING THE DISMANTLING OPERATION.
2. THE CONTRACTOR'S ENGINEER SHALL BE PRESENT DURING REMOVAL OF UPPER DECK FLOORBEAMS.
3. THIS PLAN IS NOT INTENDED TO ADDRESS TRAFFIC CONTROL, LANE LINES, K-RAIL AND OTHER SAFETY CONCERNS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING THE TRAFFIC CONTROL PLAN FOR DETAILS.
4. RIGGING SHALL CONFORM TO BEST INDUSTRY PRACTICE AND APPLICABLE REGULATIONS.
5. REFER TO CONTRACTOR'S LEAD AND ASBESTOS COMPLIANCE PLANS, HEALTH AND SAFETY PLAN, DUST CONTROL PLAN AND OTHER PLANS WHICH MAY APPLY TO THE WORK.

6. FORTHILLS BRIDGE CO. WILL NOT SUPERSEDE OR CONTRADICT THE REMOVAL OPERATIONS (OR BE RESPONSIBLE) UNLESS THE CONTRACTOR CAN PROVE THAT THE DISMANTLING PLAN OR THE SAFETY PRECAUTIONS AND PROGRAMS INCIDENT THERETO, OR FOR ANY FAILURE OF THE CONTRACTOR TO COMPLY WITH LAWS AND REGULATIONS APPLICABLE TO THE TURNING OF THE BRIDGE INTO A TEMPORARY BRIDGE.
7. ANY TEMPORARY BRIDGE SHALL BE REPLACED WITH SUFFICIENT TEMPORARY BRIDGE PRIOR TO REMOVAL OF THE BRIDGE.
8. REFER TO SHEET 3 FOR TRAFFIC CONTROL PLAN.
9. THE CONTRACTOR SHALL CLEAN AND THOROUGHLY INSPECT THE LOWER DECK FOR HAZARDOUS MATERIALS AND REMOVAL OF ALL TEMPORARY BRIDGE OPENINGS OR OTHER HAZARDS THAT MAY BE A RESULT OF THE BRIDGE REMOVAL PLAN.
10. THE REMOVAL OF FLOORBEAMS SHALL BE DONE IN A MANNER THAT WILL NOT CAUSE A RELEASE OF ENERGY AS THE BEAMS ARE CUT.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL TEMPORARY BRIDGE OPENINGS AND THE ALLOWABLE EQUIPMENT USE LOAD SHOWN IN THIS PLAN ARE BASED ON THE ASSUMPTION THAT THE FLOORBEAM PI BARS ARE ADEQUATELY BONDED.

- ## GENERAL DISMANTLING SEQUENCE
- PERFORM PRELIMINARY WORK DURING PARTIAL LANE CLOSURES (SEE NOTE 3):
- 1. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 - 2. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 - 3. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 - 4. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 - 5. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 - 6. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 - 7. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 - 8. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 - 9. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 - 10. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
- STEPS 0-10 SHALL BE PERFORMED DURING A FULL BRIDGE CLOSURE (SEE NOTE 3):
1. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 2. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 3. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 4. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 5. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
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 9. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).
 10. REMOVE 1 FLOORBEAM UNIT, 2 EDGE ORDER UNITS AND 4 COLUMNS (REFER TO SHEET 1 FOR DETAILS).

SFOBB TEMPORARY BYPASS

WEST TIE-IN DISMANTLING - STAGE 2

GENERAL INFORMATION

FILE NAME:	121-10	DATE:	05/14/2007	SHEET	1	OF	16
REVISION NO.	0						

SILVERADO CONTRACTORS, INC.

3323 PINALTA BLVD, OAKLAND, CA 94608
PHONE: 510.658.8960 FAX: 510.658.8961

FOOTHILLS BRIDGE

Ph: (203) 440.3088
Fax: (203) 93.0142
www.foothillsbridge.com

REV	DATE	DESCRIPTION
0	05/14/07	FOR REVIEW

ESTIMATED WEIGHTS

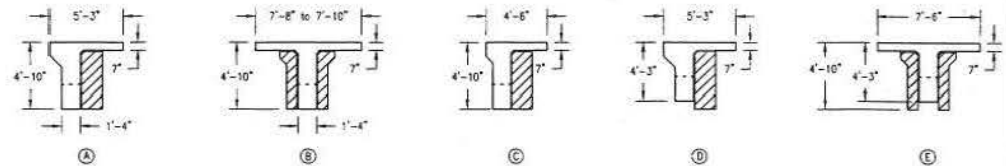
NO.	TYPE	LOCATION	LENGTH	CROSS SECTION	WEIGHT (KIPS) BEAM/GIRDER ONLY	WEIGHT (KIPS) 18" BEAM STUBS ONLY	WEIGHT (KIPS) TOTAL (KIPS)
1	FLOOR BEAM	38B	63.5'	D	123.5 KIPS		123.5 KIPS
2	FLOOR BEAM	39	66.8'	E	148.4 KIPS		148.4 KIPS
3	FLOOR BEAM	39A	66.8'	E	148.4 KIPS		148.4 KIPS
4	FLOOR BEAM	39B	63.5'	E	148.4 KIPS		148.4 KIPS
5	FLOOR BEAM	39C	66.8'	E	148.4 KIPS		148.4 KIPS
6	FLOOR BEAM	40	66.8'	E	148.4 KIPS		148.4 KIPS
7	FLOOR BEAM	40A	66.8'	E	148.4 KIPS		148.4 KIPS
8	FLOOR BEAM	40B	66.8'	E	148.4 KIPS		148.4 KIPS
9	FLOOR BEAM	40C	66.8'	E	148.4 KIPS		148.4 KIPS
10	FLOOR BEAM	41	66.8'	E	148.4 KIPS		148.4 KIPS
11	FLOOR BEAM	41A	66.8'	E	148.4 KIPS		148.4 KIPS
12	FLOOR BEAM	41B	66.8'	E	148.4 KIPS		148.4 KIPS
13	FLOOR BEAM	41C	66.8'	E	148.4 KIPS		148.4 KIPS
14	FLOOR BEAM	42	66.8'	E	148.4 KIPS		148.4 KIPS
15	FLOOR BEAM	42A	63.5'	D	123.5 KIPS		123.5 KIPS
16	FLOOR BEAM	42B	66.4'	A	136.9 KIPS		136.9 KIPS
17	FLOOR BEAM	43	66.4'	B	157.4 KIPS		157.4 KIPS
18	FLOOR BEAM	43A	66.4'	B	158.0 KIPS		158.0 KIPS
19	FLOOR BEAM	43B	66.4'	B	157.4 KIPS		157.4 KIPS
20	FLOOR BEAM	43C	66.4'	B	157.4 KIPS		157.4 KIPS
21	FLOOR BEAM	43D	66.4'	B	157.4 KIPS		157.4 KIPS
22	FLOOR BEAM	43E	66.4'	B	158.0 KIPS		158.0 KIPS
23	FLOOR BEAM	44	66.4'	B	158.6 KIPS		158.6 KIPS
24	FLOOR BEAM	44A	66.4'	B	158.0 KIPS		158.0 KIPS
25	FLOOR BEAM	44B	66.4'	B	157.4 KIPS		157.4 KIPS
26	FLOOR BEAM	44C	66.4'	B	157.4 KIPS		157.4 KIPS
27	FLOOR BEAM	44D	66.4'	B	157.4 KIPS		157.4 KIPS
28	FLOOR BEAM	44E	66.4'	B	158.0 KIPS		158.0 KIPS
29	FLOOR BEAM	45 (WEST)	66.4'	C	128.2 KIPS		128.2 KIPS
30	FLOOR BEAM	45 (EAST)	66.4'	C	128.2 KIPS		128.2 KIPS
31	FLOOR BEAM	45A	66.4'	B	158.0 KIPS		158.0 KIPS
32	FLOOR BEAM	45B	66.4'	B	157.4 KIPS		157.4 KIPS
33	FLOOR BEAM	45C	66.4'	B	157.4 KIPS		157.4 KIPS
34	FLOOR BEAM	45D	66.4'	B	157.4 KIPS		157.4 KIPS
35	FLOOR BEAM	45E	66.4'	B	158.0 KIPS		158.0 KIPS
36	FLOOR BEAM	46	66.4'	B	158.6 KIPS		158.6 KIPS
37	FLOOR BEAM	46A	66.4'	B	158.0 KIPS		158.0 KIPS
38	FLOOR BEAM	46B	66.4'	B	157.4 KIPS		157.4 KIPS
39	FLOOR BEAM	46C	66.4'	B	157.4 KIPS		157.4 KIPS
40	FLOOR BEAM	46D	66.4'	B	157.4 KIPS		157.4 KIPS
41	FLOOR BEAM	46E	66.4'	B	158.0 KIPS		158.0 KIPS
42	FLOOR BEAM	47	66.4'	B	158.6 KIPS		158.6 KIPS
43	FLOOR BEAM	47A	66.4'	B	158.0 KIPS		158.0 KIPS
44	FLOOR BEAM	47B	66.4'	B	157.4 KIPS		157.4 KIPS
45	FLOOR BEAM	47C	66.4'	B	157.4 KIPS		157.4 KIPS
46	FLOOR BEAM	47D	66.4'	B	157.4 KIPS		157.4 KIPS
47	FLOOR BEAM	47E	66.4'	B	158.0 KIPS		158.0 KIPS
48	FLOOR BEAM	48	66.4'	C	128.2 KIPS		128.2 KIPS
1	EDGE GIRDER	SOUTH 39-40A	31.0'	I	100.2 KIPS	14.0 KIPS	114.2 KIPS
2	EDGE GIRDER	SOUTH 40A-42	41.2'	I	133.1 KIPS	21.0 KIPS	154.1 KIPS
3	EDGE GIRDER	SOUTH 43-44	37.3'	H	166.6 KIPS	17.5 KIPS	184.1 KIPS
4	EDGE GIRDER	SOUTH 44-45	37.3'	H	166.6 KIPS	17.5 KIPS	184.1 KIPS
5	EDGE GIRDER	SOUTH 45-46	37.3'	H	166.6 KIPS	17.5 KIPS	184.1 KIPS
6	EDGE GIRDER	SOUTH 46-47	37.3'	H	166.6 KIPS	17.5 KIPS	184.1 KIPS
7	EDGE GIRDER	SOUTH 47-48	37.3'	H	166.6 KIPS	17.5 KIPS	184.1 KIPS
8	EDGE GIRDER	NORTH 39-40	26.0'	G	87.1 KIPS	10.5 KIPS	97.6 KIPS
9	EDGE GIRDER	NORTH 40-41	26.0'	G	87.1 KIPS	10.5 KIPS	97.6 KIPS
10	EDGE GIRDER	NORTH 41-42	26.0'	G	87.1 KIPS	10.5 KIPS	97.6 KIPS
11	EDGE GIRDER	NORTH 43-44	37.3'	F	147.5 KIPS	17.5 KIPS	165.0 KIPS
12	EDGE GIRDER	NORTH 44-45	37.3'	F	147.5 KIPS	17.5 KIPS	165.0 KIPS
13	EDGE GIRDER	NORTH 45-46	37.3'	F	147.5 KIPS	17.5 KIPS	165.0 KIPS
14	EDGE GIRDER	NORTH 46-47	37.3'	F	147.5 KIPS	17.5 KIPS	165.0 KIPS
15	EDGE GIRDER	NORTH 47-48	37.3'	F	147.5 KIPS	17.5 KIPS	165.0 KIPS

← MAX. FLOOR BEAM 158,600 LB.

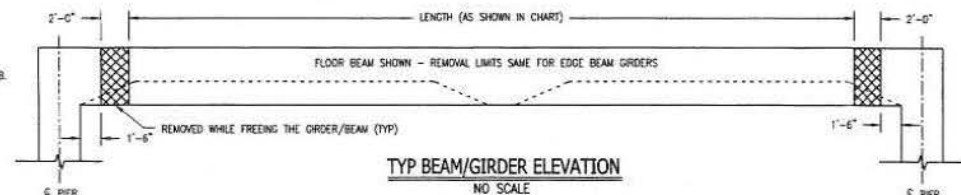
← MAX. EDGE GIRDER 184,100 LB.

NOTES

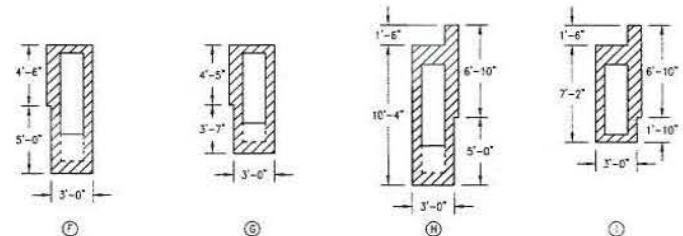
- FLOOR BEAM WEIGHTS DO NOT INCLUDE AC OVERLAY. INCREASE FLOOR BEAM WEIGHTS BY THE FOLLOWING AMOUNTS FOR EACH 1" OF AC OVERLAY REMAINING:
WIDTH 4'-6" TO 5'-3" - 4 KIPS
WIDTH 7'-6" TO 7'-10" - 6 KIPS



FLOOR BEAM SECTION
NO SCALE



TYP BEAM/GIRDER ELEVATION
NO SCALE



EDGE GIRDER SECTION
NO SCALE

1984 LIGHTWEIGHT CONCRETE
120 PCF FOR REINFORCED CONCRETE
130 PCF FOR POST-TENSIONED CONCRETE

1934 NORMAL WEIGHT CONCRETE
150 PCF

REV.	DATE	DESCRIPTION
0	05/14/06	FOR REVIEW

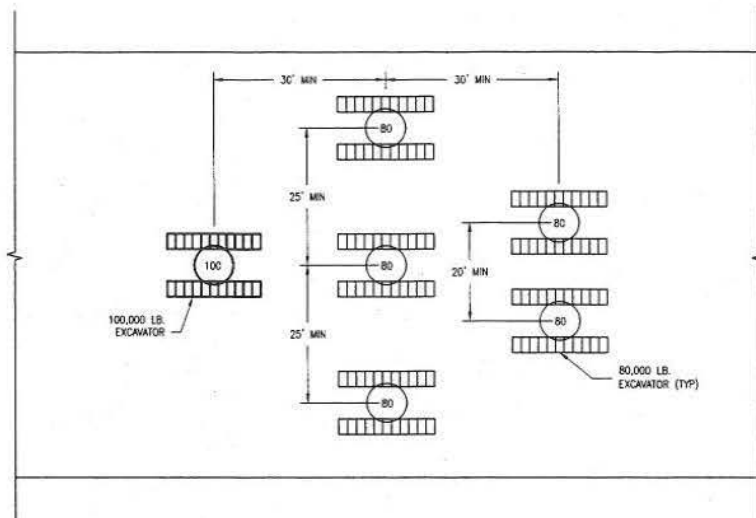


SFOBB TEMPORARY BYPASS

WEST TIE-IN DISMANTLING - STAGE 2

ESTIMATED WEIGHTS

FILENAME:	REVISION NO:	DATE:	SHEET	OF
121-08	0	05/14/2007	2	16



PLAN - ALLOWABLE UPPER DECK EXCAVATOR ARRANGEMENT

UPPER DECK EQUIPMENT LOADS

EXCAVATORS MAY OPERATE ON THE UPPER DECK WITH THE FOLLOWING REQUIREMENTS:

ALL MACHINES SHALL BE SPACED 30' MIN. ON CENTER LONGITUDINALLY.

NO MORE THAN FOUR MACHINES SHALL BE PRESENT IN ANY ONE SPAN.

THREE 80,000 LB. MACHINES MAY BE LOCATED IN A TRANSVERSE LINE SPACED AT 20' MIN. ON CENTER

OR TWO 80,000 LB. MACHINES MAY BE LOCATED IN A TRANSVERSE LINE SPACED AT 25' MIN. ON CENTER

OR ONE 100,000 LB. MACHINE MAY BE LOCATED IN A TRANSVERSE LINE.

TOTAL OTHER LIVE LOAD SHALL BE LIMITED TO 10,000 LB IN LINE WITH THE EXCAVATORS.

TRANSPORT VEHICLES SHALL CONFORM TO LEGAL LOAD LIMITS OR CALTRANS APPROVED PERMIT LOADS.

LOWER DECK EQUIPMENT LOADS

EXCAVATORS MAY OPERATE ANYWHERE ON THE LOWER DECK.

LIEBHERR LR 1400/2 CRAWLER CRANE MAY OPERATE ANYWHERE ON THE LOWER DECK.

TRANSPORT VEHICLES SHALL CONFORM TO LEGAL LOAD LIMITS OR CALTRANS APPROVED PERMIT LOADS.

REV	DATE	DESCRIPTION
0	05/14/07	FDR REVIEW



SILVERADO CONTRACTORS, INC.

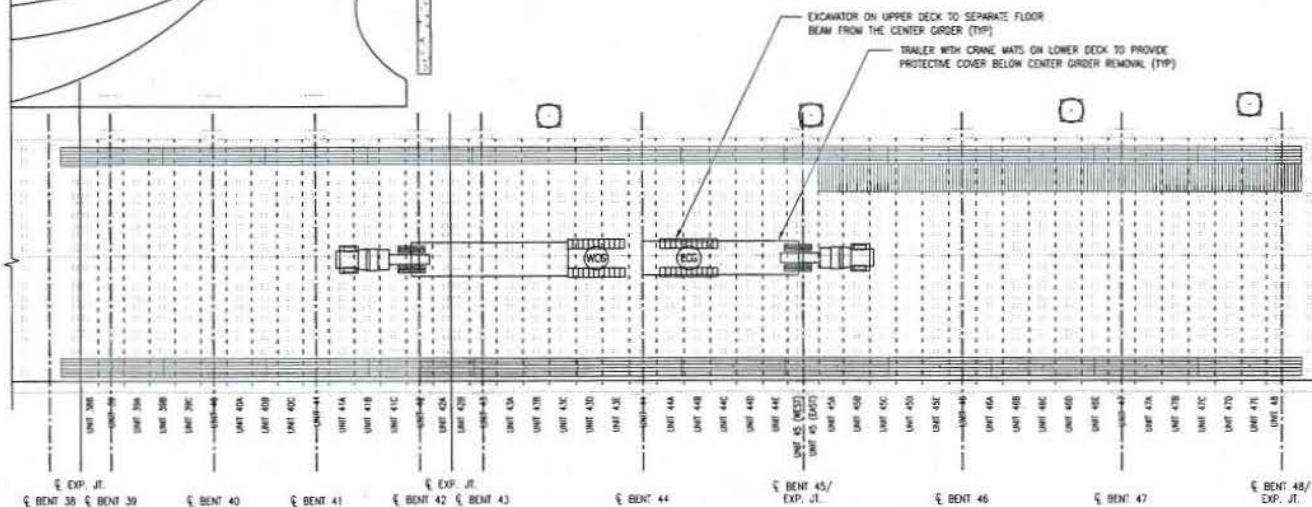
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SFOBB TEMPORARY BYPASS

WEST TIE-IN DISMANTLING - STAGE 2

ALLOWABLE EQUIPMENT LOADS

FILENAME	REVISION NO.	DATE	SHEET	OF
121-09	0	05/14/2007	3	16



PART PLAN - UPPER DECK
NO SCALE

DISMANTLING SEQUENCE STEP 0

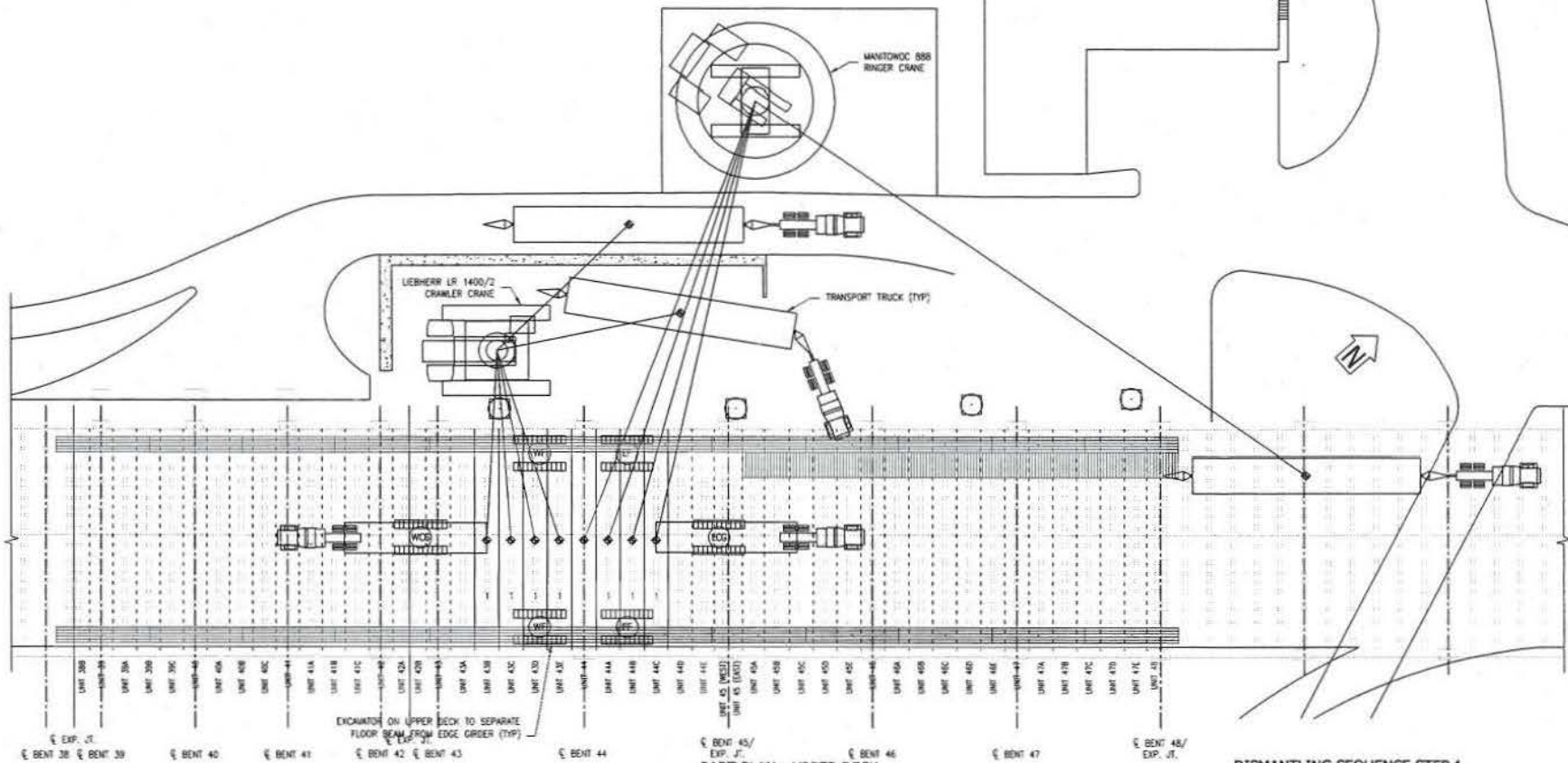
- PLACE 5' PROTECTIVE COVER MATS.
- PLACE 3/4" PLYWOOD PROTECTIVE COVER AS REQUIRED IN OTHER FALL ZONES.
- SAWCUT BETWEEN UNITS 43E AND 43B (WEST), AND BETWEEN 44 AND 44C (EAST) IN PREPARATION FOR STEP 1 REMOVAL.
- REMOVE NORTH AND SOUTH TEMPORARY K-RAIL.
- USE EASTERN CENTER GIRDER EXCAVATOR (ECG) TO BREAK THE CENTER GIRDER BETWEEN UNITS 44 AND 44C (EAST) IN PREPARATION FOR STEP 1 REMOVAL.
- USE WESTERN CENTER GIRDER EXCAVATOR (WOG) TO BREAK THE CENTER GIRDER BETWEEN UNITS 43E AND 43B IN PREPARATION FOR STEP 1 REMOVAL.

REV	DATE	DESCRIPTION
0	05/14/07	FOR REVIEW

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WEST TIE-IN DISMANTLING - STAGE 2			
DISMANTLING STEP 0			
FILENAME: 121-06	REVISION NO: 0	DATE: 05/14/2007	SHEET OF 5 16



PART PLAN - UPPER DECK
NO SCALE

DISMANTLING SEQUENCE STEP 1

- USE MANTOWOC 888 RINGER CRANE AND EASTERN FLOORBEAM EXCAVATORS (EF) TO REMOVE UNITS 44 THROUGH 44C.
- USE LIEBHERR LR 1400/2 CRANLER CRANE AND WESTERN FLOORBEAM EXCAVATORS (WF) TO REMOVE UNITS 43E THROUGH 43B.
- SAWCUT BETWEEN UNITS 44D AND 45 (EAST), AND BETWEEN 43A AND 42 IN PREPARATION FOR STEP 2 REMOVAL.
- USE EASTERN CENTER GIRDER EXCAVATOR (ECG) TO BREAK THE CENTER GIRDER BETWEEN UNITS 44D AND 45 (EAST) IN PREPARATION FOR STEP 2 REMOVAL.
- USE WESTERN CENTER GIRDER EXCAVATOR (WCG) TO BREAK THE CENTER GIRDER BETWEEN UNITS 43A AND 42 IN PREPARATION FOR STEP 2 REMOVAL.

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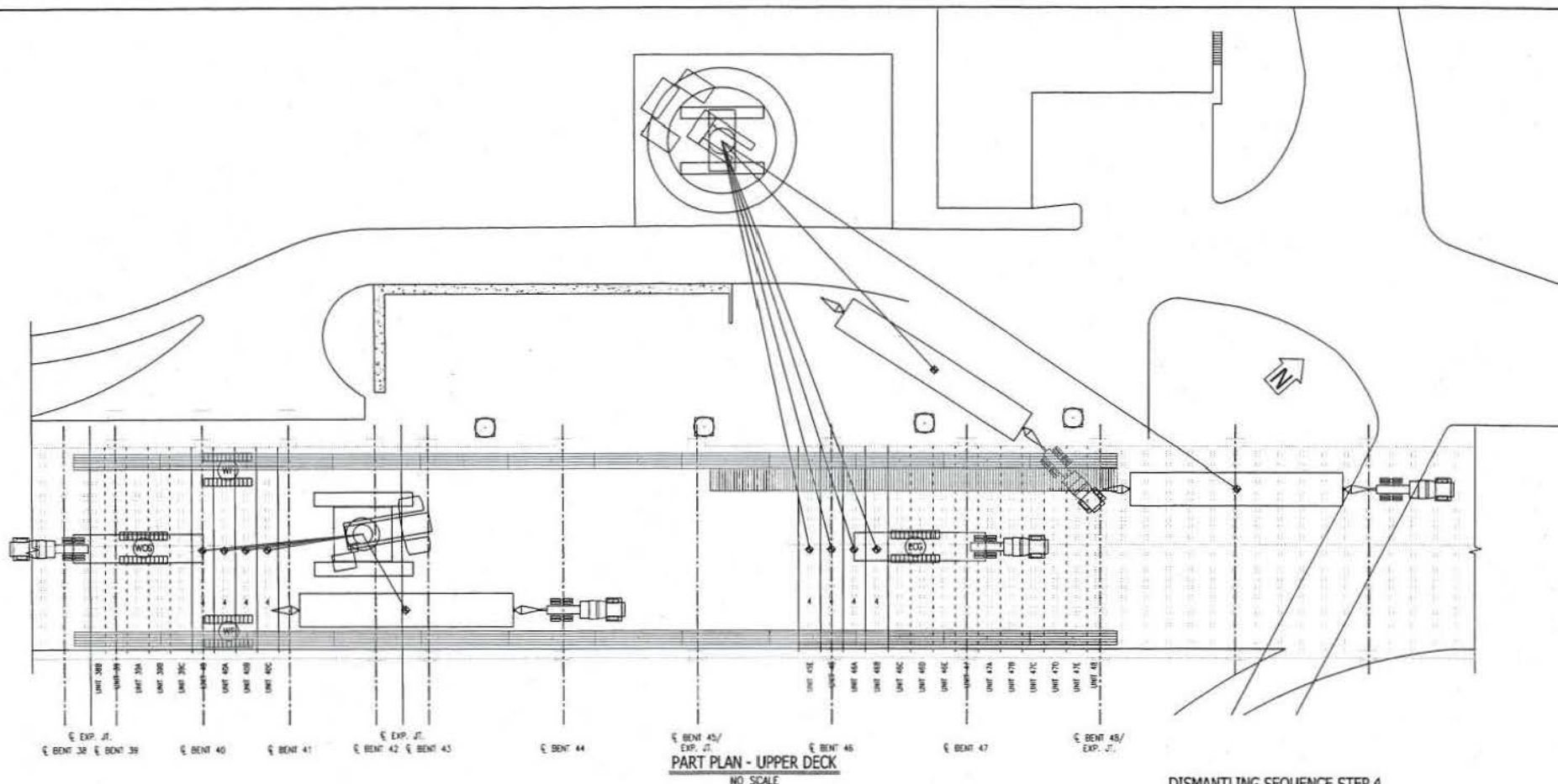
SFOBB TEMPORARY BYPASS			
WEST TIE-IN DISMANTLING - STAGE 2			
DISMANTLING STEP 1			
FILENAME: 121-06	REVISION NO: 0	DATE: 05/14/2007	SHEET OF 6 16



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DISMANTLING SEQUENCE STEP 4

- USE MANTOWOC 880 RINGER CRANE AND EASTERN FLOORBEAM EXCAVATORS (EF) TO REMOVE UNITS 45E THROUGH 48E.
- USE LIEBHERR LR 1400/2 CRAWLER CRANE AND WESTERN FLOORBEAM EXCAVATORS (WF) TO REMOVE UNITS 40C THROUGH 40.
- SANGUIT BETWEEN UNITS 39C AND 38B IN PREPARATION FOR STEP 5 REMOVAL.
- USE WESTERN CENTER GARDER EXCAVATOR (WCG) TO BREAK THE CENTER GARDER BETWEEN UNITS 38C AND 38B IN PREPARATION FOR STEP 5 REMOVAL.

REV	DATE	DESCRIPTION
0	05/14/07	FOR REVIEW


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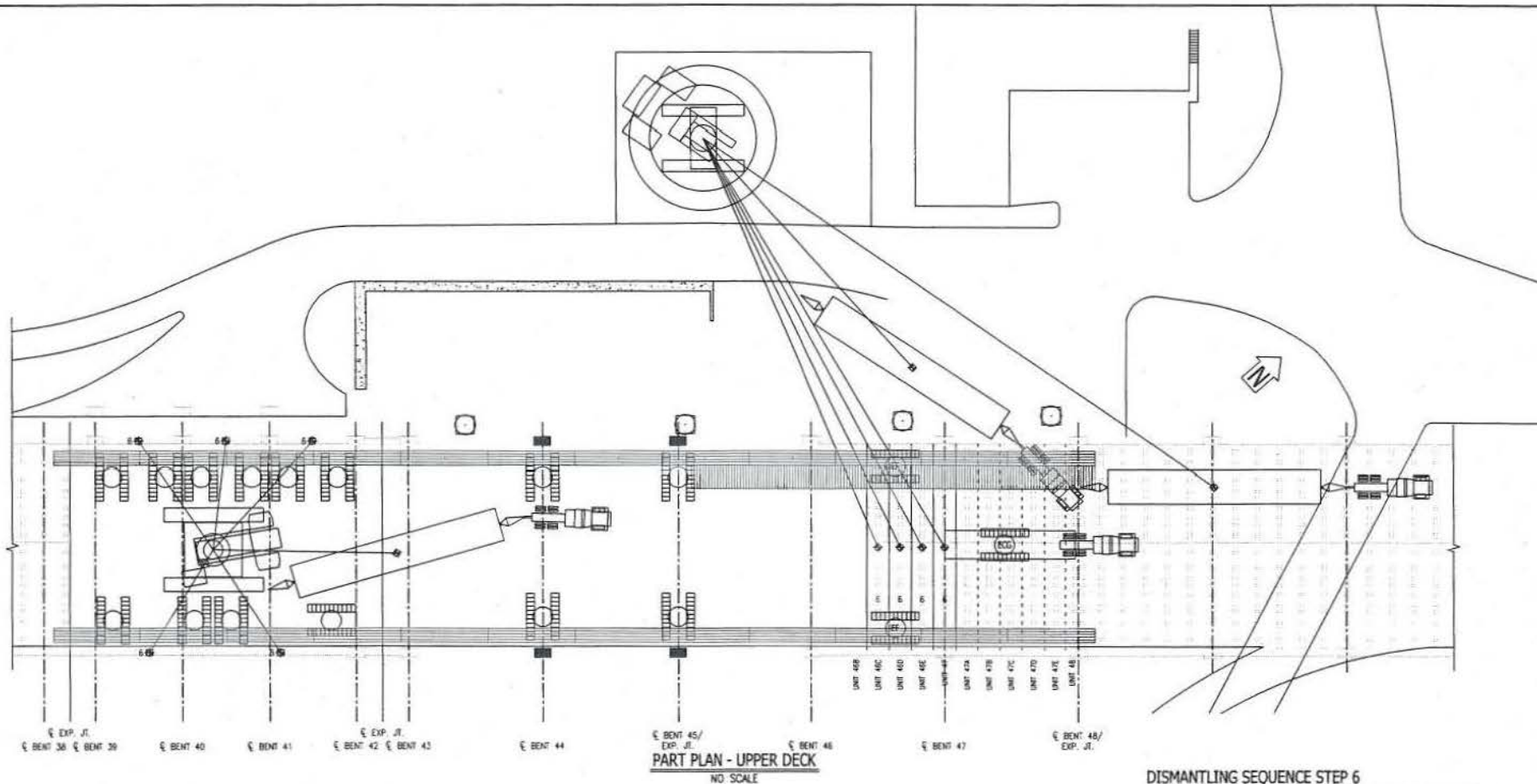
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SFOBB TEMPORARY BYPASS

WEST TIE-IN DISMANTLING - STAGE 2

DISMANTLING STEP 4

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121-06	0	05/14/2007	9	16



DISMANTLING SEQUENCE STEP 6

- USE WANTWOC 888 RINGER CRANE AND EASTERN FLOORBEAM EXCAVATORS (EF) TO REMOVE UNITS 45B THROUGH 47.
- USE LIEBHERR LR 1400/2 CRAWLER CRANE AND EXCAVATORS TO REMOVE NORTH ORDERS 42-41, 41-40, AND 40-39, AND SOUTH ORDERS 42-40A AND 40A-39.
- USE EXCAVATORS TO REMOVE NORTH AND SOUTH COLUMNS 44 AND 45.
- SAWCUT BETWEEN UNITS 47A AND 48 IN PREPARATION FOR STEP 7 REMOVAL.
- USE EASTERN CENTER ORDER EXCAVATOR (ECG) TO BREAK THE CENTER ORDER BETWEEN UNITS 47A AND 48 IN PREPARATION FOR STEP 7 REMOVAL.

REV	DATE	DESCRIPTION
0	05/14/07	FOR REVIEW



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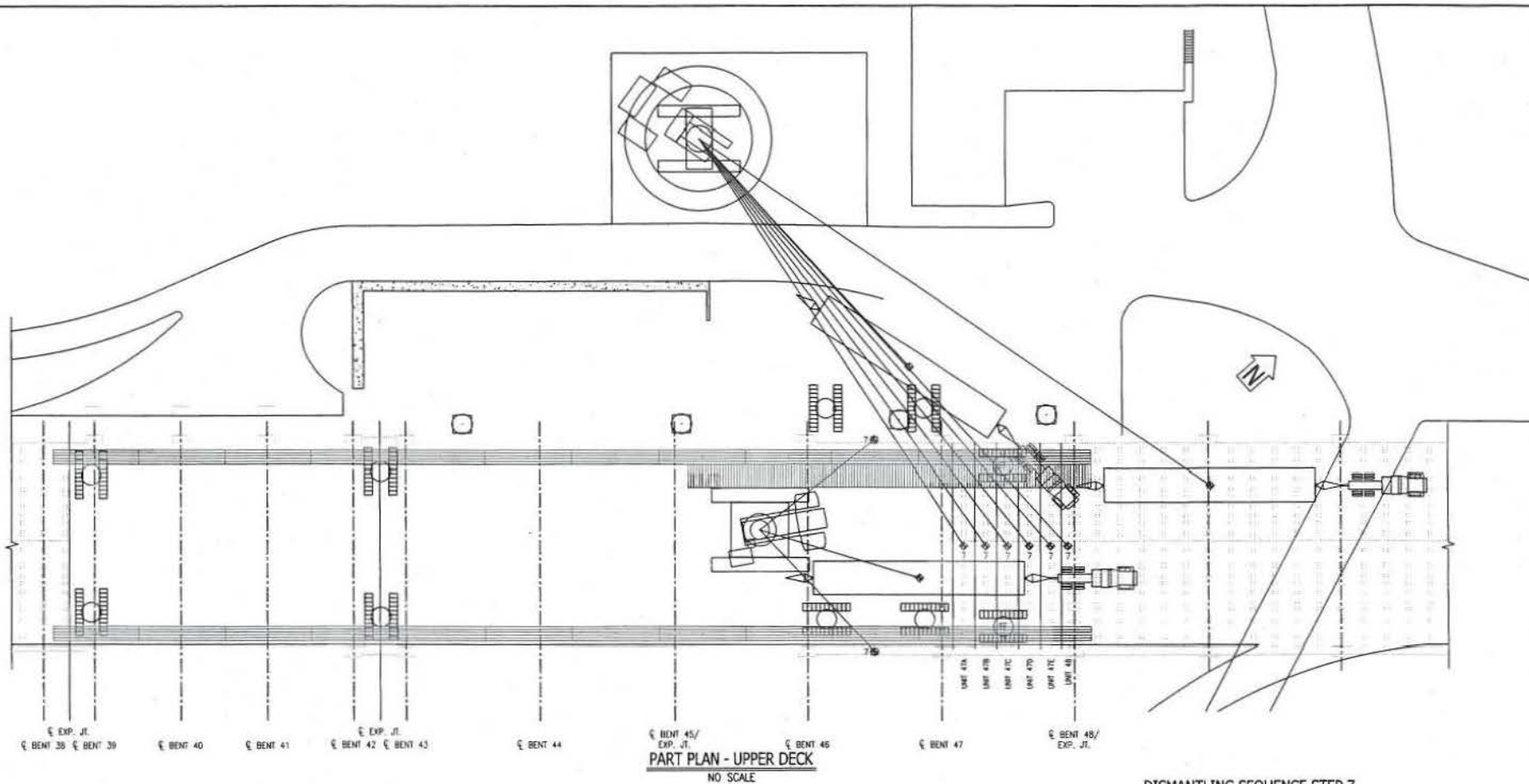
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WEST TIE-IN DISMANTLING - STAGE 2

DISMANTLING STEP 6

FILENAME	REVISION NO.	DATE	SHEET	OF
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DISMANTLING SEQUENCE STEP 7

- USE HANITOWOC 888 RINGER CRANE AND EASTERN FLOORBEAM EXCAVATORS (EF) TO REMOVE UNITS 47A THROUGH 48.
- USE LIEBHERR LR 1400/2 CRAWLER CRANE AND EXCAVATORS TO REMOVE NORTH AND SOUTH GIRDERS 46-47.
- USE EXCAVATORS TO REMOVE NORTH AND SOUTH GIRDER CANTILEVERS 38-HINGE, 42-HINGE AND 43-HINGE.

REV	DATE	DESCRIPTION
0	05/14/07	FOR REVIEW



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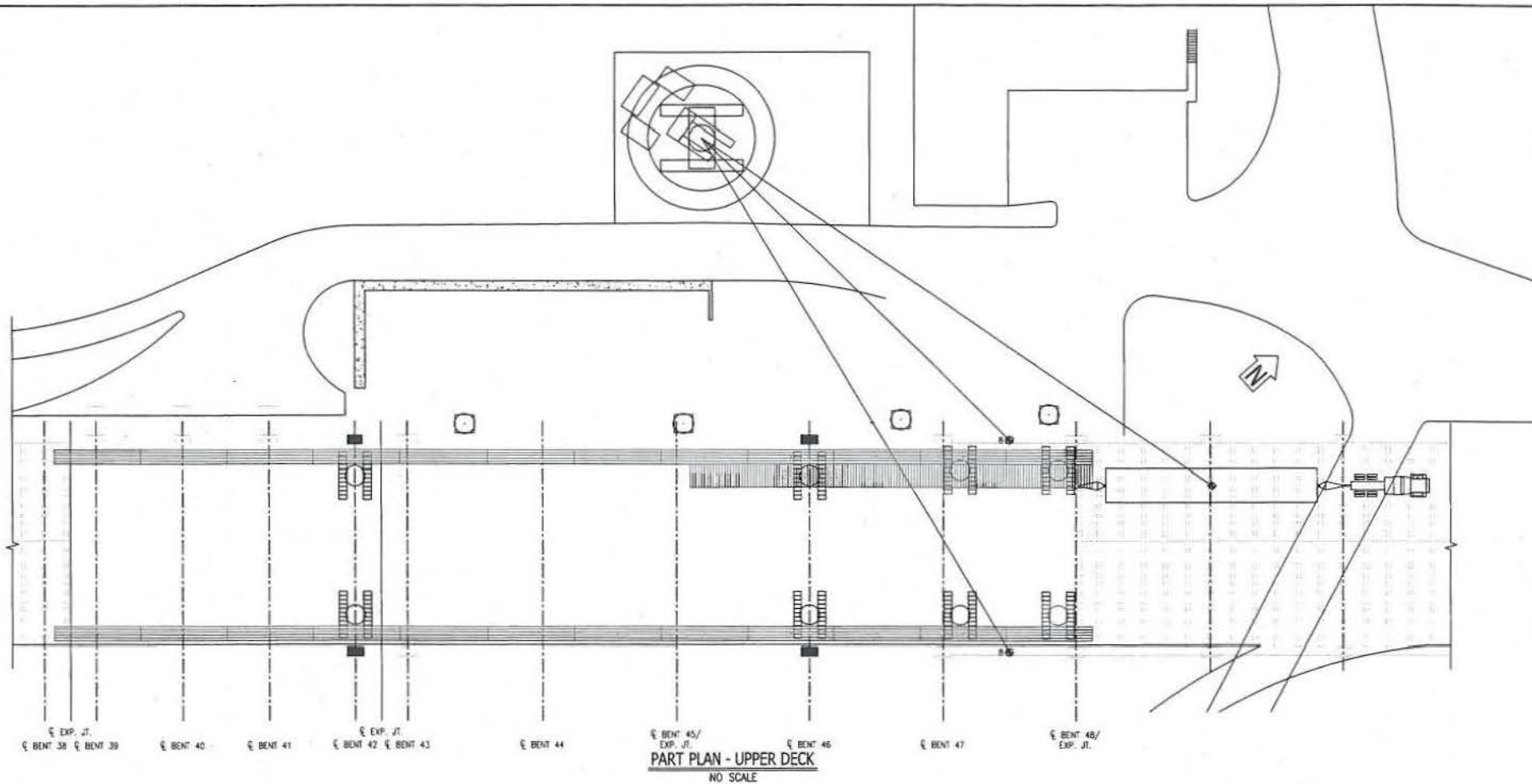
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WEST TIE-IN DISMANTLING - STAGE 2

DISMANTLING STEP 7

FILENAME	REVISION NO	DATE	SHEET	OF
121-06	0	05/14/2007	12	16



DISMANTLING SEQUENCE STEP 8

- USE MANTOWOC 888 RINGER CRANE AND EXCAVATORS TO REMOVE NORTH AND SOUTH GIRDERS 47-48.
- USE EXCAVATORS TO REMOVE NORTH AND SOUTH COLUMNS 46 AND 42.

REV	DATE	DESCRIPTION
0	05/14/07	FOR REVIEW



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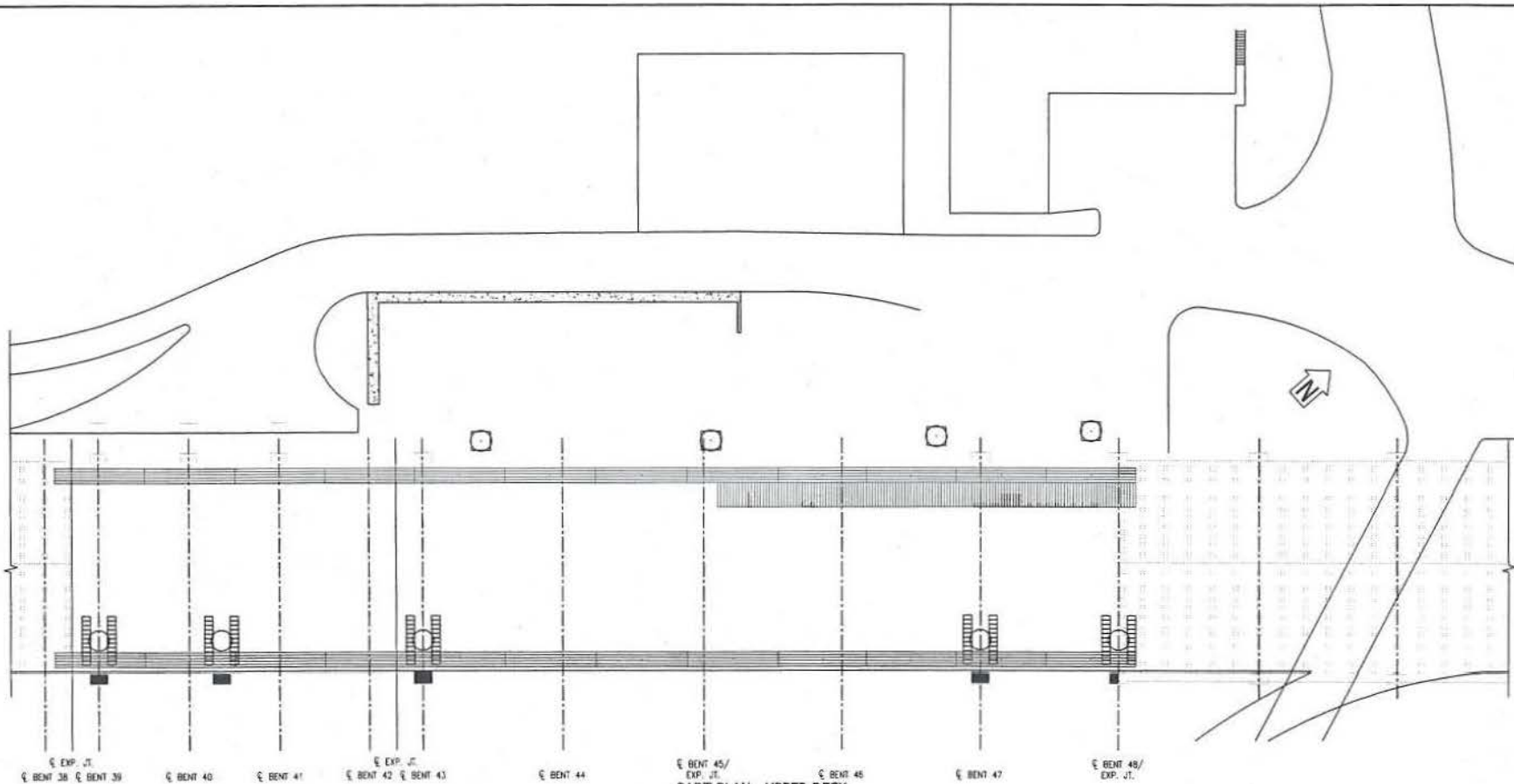
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WEST TIE-IN DISMANTLING - STAGE 2

DISMANTLING STEP 8

FILENAME:	REVISION NO:	DATE:	SHEET	OF
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EXP. JT. BENT 38 BENT 39 BENT 40 BENT 41 EXP. JT. BENT 42 BENT 43 BENT 44 BENT 45/EXP. JT. BENT 46 BENT 47 BENT 48/EXP. JT.

PART PLAN - UPPER DECK
 NO SCALE

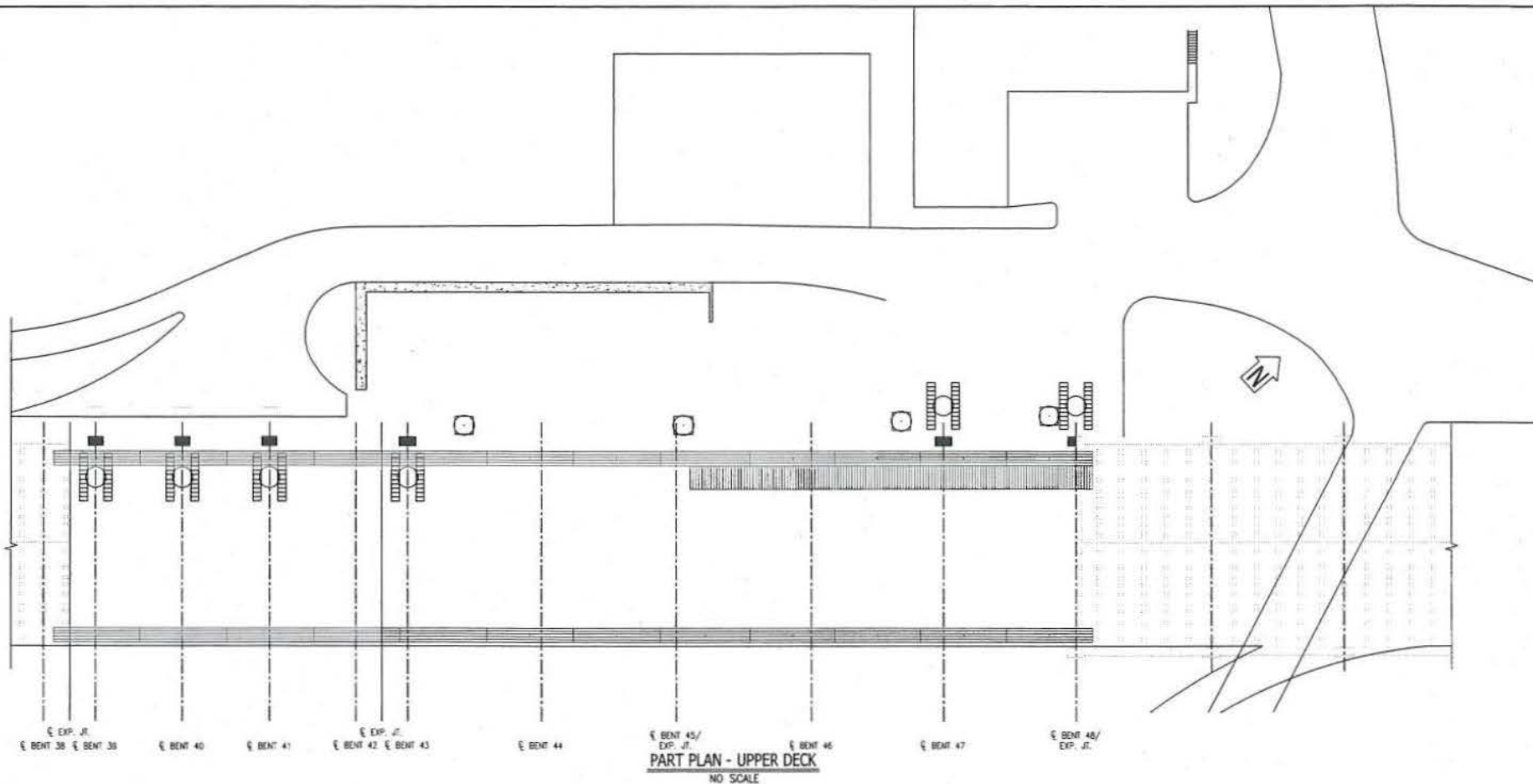
DISMANTLING SEQUENCE STEP 9
 * USE EXCAVATORS TO REMOVE SOUTH COLUMNS 48, 47, 43, 40A, AND 39.

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0	05/14/07	FOR REVIEW


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SFOBB TEMPORARY BYPASS			
WEST TIE-IN DISMANTLING - STAGE 2			
DISMANTLING STEP 9			
FILENAME: 121-06	REVISION NO: 0	DATE: 05/14/2007	SHEET OF 14 16



DISMANTLING SEQUENCE STEP 10

• USE EXCAVATORS TO REMOVE NORTH COLUMNS 46, 47, 43, 41, 40, AND 39.

REV	DATE	DESCRIPTION
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WEST TIE-IN DISMANTLING - STAGE 2

DISMANTLING STEP 10

FILENAME:	REVISION NO:	DATE:	SHEET	OF
121-06	0	05/14/2007	15	16

- Item 2: San Francisco-Oakland Bay Bridge**
 - b. Yerba Buena Island**
 - 1) CCO Implementation Strategy
for SSD**

TO: Toll Bridge Program Oversight Committee DATE: June 21, 2007
(TBPOC)

FR: Tony Anziano, Toll Bridge Program Manager, Caltrans

RE: Agenda No. - 2b1

Item- San Francisco-Oakland Bay Bridge
CCO Implementation Strategy for South-South Detour

Recommendation:

For information only

Discussion:

Attached is a Contract Change Order Implementation Strategy for South-South Detour that builds on and provides an update to a similar document presented to the TBPOC at the May 1, 2007 meeting.

The purpose of this document is to provide a status of the construction budget, and serves as a check between CCO expenditures, estimates developed in the strategy memos, and the approved funding for the project.

Additional information for the YBI CCO's will be sent under separate cover.

Attachment(s):

- 1) CCO Implementation Strategy for South-South Detour, June 21, 2007
- 2) Strategy and Scope exhibit, December 13, 2006
- 3) YBITS Advance exhibit, December 14, 2006

**Contract Change Order Implementation Strategy for South-South Detour
04-0120R4
June 22, 2007**

DRAFT

South-South Detour (Contract 04-0120R4)			
Contract Award:	March 10th, 2004	Suspension Days (as of 04/13/07):	572 Working Days
Original Working Days:	475 Working Days	Contract Extensions (as of 04/13/07):	381 Working Days
Original Contract Completion:	July 27th, 2005	Projected Contract Completion:	November 26, 2009
Original Contract Amount:	\$71,159,650	Projected Contract Cost:	\$334,400,000

Introduction

Two memos were developed to outline a strategy for a revised SSD project that enhanced SSD viaduct design, developed tie-in design (east and west) in-house, improved the retrofit of the YBI viaduct (replacing the top deck of the viaduct rather than retrofitting in place) and advanced and incorporated select YBITS foundation work. The two memos are “*San Francisco-Oakland Bay Bridge Corridor Schedule Mitigation – Strategy for South-South Detour Contract Completion*” issued December 14, 2006, and “*Recommendation to Construct Select Yerba Buena Island Transition Structure Foundations by Contract Change Order*” issued on December 25, 2006. This strategy will result in substantial increases in the cost of the SSD project. The SSD forecast and budget were recently revised and the current forecast and budget have been set at \$ 334 million. This figure was based on estimates developed and presented in the two strategy memos as well as the original contract amount, pre-existing contract change orders (CCO) and a contingency/risk management adjustment.

The purpose of this document is to provide a status of the construction budget, and serves as a check between CCO expenditures, estimates developed in the strategy memos and the approved funding for the project.

Scope of Work for SSD

The revisions to the original scope of work currently associated with the South-South Detour Project have been broken down into the following categories:

- (1) SSD New Viaduct Enhancements
- (2a) West Tie-In Existing Viaduct Phase 1
- (2b) West Tie-In Phase 2
- (3) East Tie-In
- (4) YBI Transition Structure Advance Foundations
- (5) Administrative

An exhibit showing these categories and the general construction limits can be found in the included attachments.

DRAFT

The current total estimate for CCOs required to modify the original scope of SSD work in these defined categories is \$ 245 million. This estimate is based on more detailed analysis than was available during preparation of the strategy memos and in many cases includes auditable input from the contractor as well as independent verification from Bay Area Management Consultants. The estimate in the two strategy memos for this work was \$ 255 million. Some categories have increased while others have decreased. The current estimate for the SSD contract, including the modifications to the scope of work is \$ 329 million, \$ 5 million below the original estimate of \$ 334 million. This current estimate consists of the following:

Original Contract Amount	\$ 71.2 million
Baseline CCOs (1 through 48)	\$ 12.1 million
State Furnished Materials	\$ 0.4 million
Strategy memo CCOs (49 and higher)	\$ 245.4 million
Total	\$ 329.1 million

Current estimates for the categories of work established in the strategy memo CCOs are addressed separately in the following sections.

SSD New Viaduct

1

Progress of Work

Construction of foundations and columns on the SSD bridge has been ongoing since early on in the project. Currently, all viaduct foundations are complete and the Contractor is constructing the remaining columns. Due to the revised strategy and design changes, the new viaduct structure was made to be a stand-alone structure. To accommodate this, bent caps were added between the tops of each pair of columns. In March 2007, the Contractor began erecting the falsework in preparation of retrofitting the columns and constructing the bent caps. This work is ongoing.

Fabrication of structural steel truss for the viaduct superstructure is currently taking place at Dongkuk S&C in South Korea. This fabrication began in November 2006 with the first deliveries to the project expected to arrive in October 2007. As of May 16, 2007, fabrication is approximately 41% complete.

DRAFTStatus of Contract Change Orders: SSD New Viaduct

CCO	Method of Payment	Description	Plans from Design	CT Estimate Complete	CCM Estimate Complete	HQ ATP	TBPOC Approval	HQ I&A	Target TBPOC Meeting Date	CCO Executed	Anticipated CCO Cost
49	LS	Stringer and Floor Beam Design Study	N/A	Yes	Yes	N/A	ATN April 2006	N/A	TBD	5/2/2006	\$109,000
49S1	FA	Truss Design Modifications (Changes to Stringer and Floor Beam Connections)	N/A	Yes	Yes	N/A	ATN April 2006	Yes 12/08/06	TBD	8/17/2006	\$150,000
49S2	FA		N/A	Yes	Yes					12/18/2006	\$100,000
Subtotal (CCO #49 and Supplements)											\$359,000
50	FA	Stand Alone Viaduct Design	N/A	Yes	Yes	N/A	ATN April 2006	Yes 12/08/06 2/09/07	TBD	5/8/2006	\$325,000
50S1	FA			Yes	Yes					10/16/2006	\$300,000
50S2	FA			Yes	Yes					12/18/2006	\$100,000
50S3	FA			Yes	Yes					2/13/2007	\$175,000
Subtotal (CCO #50 and Supplements)											\$900,000
55		Steel Fabricator Change (SGT Closeout)	N/A	Yes	Yes	N/A	ATP April 2006	No	6/27/2007	No	\$5,665,330
60		Construction of Bent Caps	N/A	Yes	Yes	Yes	ATN May 2007	No	6/27/2007	No	\$7,435,950
67		Viaduct/ETI Interface Modifications	N/A	Yes	No	N/A	N/A	Yes	N/A	No	\$400,000
79		Fabrication Cost for Viaduct Design Changes	N/A	No	Yes	No	Pending	No	8/2/2007	No	\$8,000,000
80		Erection Costs for Viaduct Design Changes	N/A	No	No	No	Pending	No	8/2/2007	No	\$4,000,000
82		AC Paving and Erosion Control for Deck	No	No	No	N/A	N/A	N/A	N/A	No	\$250,000
Current Forecast for SSD New Viaduct											\$27,010,280

Bold = CCO's not issued yet

ATN = Authorization to Negotiate

ATP = Authorization to Proceed

Additionally, costs for changing steel fabricators (CCO #55) had been originally estimated to be \$10.5M. This issue has been negotiated with the Contractor and CCO #55 is anticipated to be issued for approximately \$5.665M. Contract Change Orders #79, #80 and #82 have recently been assigned. CCO #79 will address fabrication costs resulting from the Viaduct design changes while CCO #80 will address steel erection costs resulting from the Viaduct design changes. CCO #82 will address AC Paving and Erosion Control Issues resulting from Department ordered changes to the Viaduct drainage scheme.

Changes Since Last Report

No changes to report.

DRAFT

Budget Status

The Viaduct portion of the SSD was bid at \$26.74M. The projected additional costs in the December 14, 2006 Strategy Memorandum were estimated to be \$9M. Currently the total additional costs associated with viaduct enhancements are approximately \$27.0M. This included approximately \$1.26M that has been allocated to Contract Change Orders #49 and #50 and approximately \$20.1M estimated for various related construction costs. In April 2006, the TBPOC approved \$1.0M for CCO's #49 and #50 and \$4.0M for the related construction. Finalized costs will be provided once negotiations are complete. The TBPOC also approved authority to negotiate in the amount of \$8.5M for the relocation of Viaduct fabrication from China to South Korea. The originally estimated \$10.5M in closeout cost has been negotiated down to approximately \$5.665M. These added entitlement costs will be paid from previously approved supplemental funds. Additionally, in May 2007, the TBPOC approved authority to negotiate in the amount of \$8.0M for construction of bent caps (CCO #60).

West Tie-In Existing Viaduct

Phase 1

2a

Progress of Work

Phase 1 construction in the West Tie-In area began in January 2007 with clearing and grubbing on the south side of the existing bridge. The Contractor has completed excavation and construction of retaining walls for the staging area and the retrofit of existing outrigger column 40A. On the north side of the existing structure, demolition of the existing bridge overhang and installation of CIDH piles and column for the new West Tie-In Viaduct structure has been completed and construction of columns is proceeding. CIDH piles on the south side have also been completed and construction of columns, skid bent foundations, and falsework are proceeding.

Planning for the Labor Day Closure is ongoing. The Department has been working closely with the Contractor and its demolition subcontractor to address contingency plans and ways of ensuring that work proceeds as planned during the closure. Weekly meetings are being held to address TMP issues and a Media Outreach was held on April 11th, 2007 to kickoff the media awareness campaign.

Contract Change Order Implementation Strategy for South-South Detour

June 22, 2007

DRAFT

Status of Contract Change Orders: West Tie-In Existing Viaduct (Phase 1)

CCO	Method of Payment	Description	Plans from Design	CT Estimate Complete	CCM Estimate Complete	HQ ATP	TBPOC Approval	HQ I&A	Target TBPOC Meeting Date	CCO Executed	Anticipated CCO Cost
57S1		Remove and Clear Building 254	N/A	Yes	Yes	No	N/A	No	N/A	No	\$10,572
61	FA	Advance Engineering (Work Plans and Submittals), Site Prep (Ramp Closures, Access Road), Civil Work (Grading), Structure Work (Material Procurement)	Yes	Yes	N/A	Yes	N/A	Yes 1/09/07	N/A	2/27/2007	\$400,000
61S1	LS/FA	Construction of Stage 1 Area and Substructure	Yes	Yes	Yes	Yes	ATN May 2007	Yes 5/16/07	6/27/2007	5/18/2007	\$9,995,644
Subtotal (CCO #61 and Supplements)											\$10,395,644
66		TMP - Video Equipment (WTI Phase 1)	No	Yes	No	No	N/A	No	N/A	No	\$90,000
68	FA	Temporary Electrical Work	No	Yes	N/A	N/A	N/A	N/A	N/A	No	\$140,000
72	LS	Structure Work (Superstructure), and Temporary Shuttle Service	Yes	Yes	Yes	Yes	Pending	No	6/27/2007	No	\$12,000,000
74		TMP - Labor Day Planning and Implementation (WTI Phase 1)	No	No	No	N/A	Pending	No	8/2/2007	No	\$2,000,000
76	LS	Labor Day Bridge Demolition and Move-In	Yes	No	No	Yes	Pending	No	8/2/2007	No	\$6,000,000
81		Relocate Palm Tree	N/A	No	No	N/A	N/A	N/A	N/A	No	\$10,000
84		Skid Track Foundations and Temporary	N/A	No	Yes	No	Pending	No	8/2/2007	No	\$4,140,600
Current Forecast for West Tie-In Existing Viaduct											\$34,786,816

Bold = CCO's not issued yet
 ATN = Authorization to Negotiate
 ATP = Authorization to Proceed

Costs for the demolition of USCG Building 254 have been agreed to and CCO #57S1 is being processed. The various CCO's for Phase 1 West Tie-In work are currently being negotiated with the Contractor. A temporary shuttle service to address public access impacted by construction of the West Tie-In is in place.

Changes Since Last Report

No changes to report.

DRAFTBudget Status

The estimated cost of adding the Phase 1 West Tie-In work is \$40M. Estimates are currently being updated as they are finalized and will be included in future updates. The TBPOC approved authority to negotiate in the amount of \$10M for CCO #61S1 at the May 2007 TBPOC Meeting. The Department has since agreed to a \$9.995M lump sum price for CCO #61S1. Approximately \$34.8M is currently forecast for the various West Tie-In (Phase 1) CCO's.

West Tie-In**Phase 2****2b**Progress of Work

All design for the Phase 2 portions of the West Tie-In will be completed by January 2008. Portions of the final design such as foundations and substructure elements will be provided to the Contractor as they become available. Construction of foundations for the Phase 2 West Tie-In is scheduled to begin after the completion of the Phase 1 West Tie-In work after Labor Day Weekend 2007.

Status of Contract Change Orders: West Tie-In (Phase 2)

CCO	Method of Payment	Description	Plans from Design	CT Estimate Complete	CCM Estimate Complete	HQ ATP	TBPOC Approval	HQ I&A	Target TBPOC Meeting Date	CCO Executed	Anticipated CCO Cost
52	N/A	Elimination of Contractor's Design of Tie-Ins	N/A	N/A	N/A	Yes	N/A	Yes 1/19/07	N/A	3/2/2007	\$0
57		Demolition of Building 206	N/A	Yes	Yes	N/A	N/A	N/A	N/A	10/18/2006	\$22,378
62		Construction of Phase 2 WTI	No	No	No	No	Pending	No	8/2/2007	No	\$13,000,000
TBD		Design Modification to accommodate Phase 1 WTI Plans	No	No	No	No	Pending	No	8/2/2007	No	\$500,000
Current Forecast for West Tie-In											\$13,522,378

Bold = CCO's not issued yet

ATN = Authorization to Negotiate

ATP = Authorization to Proceed

CCO #52 has been executed at no cost to address designer of record issues related to the Department taking back the design of the East and West Tie-In. Cost related to construction is currently estimated at \$13.5M and will be addressed in the construction related CCO's for the individual elements.

DRAFTChanges Since Last Report

No changes to report.

Budget Status

The Contractor's bid price for the West Tie-In was \$9.0M. Based on the Department's Strategy Memorandum, the costs associated with the Phase 2 West Tie-In work were estimated to be an additional \$13M to the original contract bid item.

East Tie-In**3**Progress of Work

The 35% Design was submitted by TY Lin on April 17, 2007. A completed design is anticipated second quarter 2008. Portions of the final design such as foundations and substructure work will be provided to the Contractor as it becomes available with portions of the construction anticipated to begin in late 2007. Prior to the ETI work starting, a pump station owned by the City of San Francisco will be relocated by July/August 2007. Specialized equipment/materials for the relocation has been ordered (CCO #69).

Status of Contract Change Orders: East Tie-In

CCO	Method of Payment	Description	Plans from Design	CT Estimate Complete	CCM Estimate Complete	HQ ATP	TBPOC Approval	HQ I&A	Target TBPOC Meeting Date	CCO Executed	Anticipated CCO Cost
52	N/A	Elimination of Contractor's Design of Tie-Ins	N/A	N/A	N/A	Yes	N/A	Yes 1/19/07	N/A	3/2/2007	\$0
63		Advance Engineering (Work Plans and	No	No	No	No	N/A	No	N/A	No	\$500,000
69		Procurement of Pump/Control Panel for Pump	Yes	Yes	Yes	No	N/A	No	N/A	No	\$111,280
83		Construction for Pump and Control Panel for	Yes	No	No	No	N/A	No	N/A	No	\$500,000
TBD		Site Prep and Civil Work	No	No	No	No	Pending	No	8/2/2007	No	\$5,000,000
TBD		Structure Work (Skid Bent Foundations and Substructure)	No	No	No	No	Pending	No	10/30/2007	No	\$15,000,000
TBD		Structure Work (ETI Superstructure), Bridge Removal, Utility Relocation/Removal	No	No	No	No	Pending	No	TBD	No	\$13,500,000
TBD		Utilities: Fiber Optic Line Along Shore	No	No	No	No	N/A	No	N/A	No	\$250,000
TBD		TMP - Planning and Implementation (ETI)	N/A	No	No	No	Pending	No	TBD	No	\$2,000,000
Current Forecast for East Tie-In											\$36,861,280

Bold = CCO's not issued yet
 ATN = Authorization to Negotiate
 ATP = Authorization to Proceed

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CCO #52 has been executed at no cost to address designer of record issues related to the Department taking back the design of the East and West Tie-In. The Contractor fulfilled its obligation to design the ETI. As such, the original contract allotment for this bid item will be paid and any credit to the Department will be negotiated. The changes related to construction will be addressed in the construction related CCO's for the individual elements.

Changes Since Last Report

No changes to report.

Budget Status

The work item for East Tie-In originally bid by the Contractor was \$6.0M. Additionally, another \$1.46M was bid by the Contractor for the demolition of the existing span moved out for the East Tie-In. The Department forecasts additional costs associated with the construction of the East Tie-In to be \$36.9M. As the work progresses and related Contract Change Orders are negotiated, the estimate will be updated.

Yerba Buena Island Transition Structures Advance Foundations

4

Progress of Work

The current YBITS foundation and column locations being advanced are W3R/L, W4R/L, W5R/L, W6R/L, and the W7 Ramp. Construction at Bent W3L was completed March 15th 2007 on CCO #64S1. This work consisted of constructing the footing (including tie-downs) and the column up to the splice zone. This work was accomplished on an accelerated schedule to accommodate the SAS Contractor's schedule for W2 Bent Cap construction. It is anticipated that the SSD Contractor will be able to resume work in this area and proceed with the construction of Bent W3R in January 2008. Work on Integrated Shop Drawings is currently underway. Construction of Bent W4L has begun with excavation for the footing.

DRAFTStatus of Contract Change Orders: YBI Transition Structures Advance Foundations

CCO	Method of Payment	Description	Plans from Design	CT Estimate Complete	CCM Estimate Complete	HQ ATP	TBPOC Approval	HQ I&A	Target TBPOC Meeting Date	CCO Executed	Anticipated CCO Cost
64	FA	YBITS W3L Site Prep and Grading and Construct	Yes	Yes	N/A	N/A	N/A	N/A	N/A	1/8/2007	\$150,000
64S1	LS/FA	YBITS W3L Foundation and Column to Splice Zone, Integrated Shop Drawings for W3L, Concrete Washouts, 50% of Flagging, and Traffic	Yes	Yes	Yes	Yes	ATP February 2007	Yes 3/13/07	Done	4/4/2007	\$5,835,000
Subtotal (CCO #64 and Supplements)											\$5,985,000
65		Demolition of Existing Bridge	No	No	No	No	Pending	No	TBD	No	\$3,500,000
70	FA	Integrated Shop Drawings for Remaining YBITS Advance Locations (W3R, W4L/R, W5L/R,	Yes	Yes	Yes	Yes	N/A	Yes 4/4/07	N/A	Yes	\$500,000
73	LS	YBITS W3R, W4R, W5R/L, W6R/L and W7 Ramp Foundations and Columns	Yes	Yes	Yes	No	Pending	No	6/27/2007	No	\$65,000,000
75	LS	YBITS W7R/L Foundations and Columns	No	No	No	No	Pending	No	8/2/2007	No	\$20,000,000
77	LS	YBITS W4L Foundations and Columns	Yes	Yes	Yes	No	Pending	No	6/27/2007	No	\$8,000,000
78	LS	Relocation of Sewer Force Main	Yes	Yes	Yes	N/A	N/A	N/A	N/A	No	\$125,057
Current Forecast for YBI Transition Structures Advance Foundations											\$103,110,057

Bold = CCO's not issued yet

ATN = Authorization to Negotiate

ATP = Authorization to Proceed

The Department has estimated the cost of the YBITS Advance Foundations to be \$110.5M. The Department is currently waiting for a revised cost proposal from the Contractor for CCO's related to the remaining YBITS Advance work. Final plans for the W7R/L work have recently been received from Design and forwarded to the Contractor. Removal of the existing bridge is included in the current contract. However, the Department anticipates additional costs resulting from impacts of the YBITS Advance work and associated costs due to escalation. CCO #65 has not been issued.

Changes Since Last Report

No changes to report.

Budget Status

The construction of the YBITS Advance Foundations and Columns was estimated to cost \$110.5M. The TBPOC gave approval to negotiate a CCO for work at Bent W3L up to an amount not to exceed \$7M. Contract Change Orders #64 and #64S1 have been issued for a total of \$5.985M. Costs associated with CCO #73 and #77 are currently being negotiated with the Contractor. The Contractor's bid price for demolition of the main bridge structure is \$3.5M. The added costs associated with demolition of the existing

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structure were forecast to be another \$3.5M. As the work progresses and the related Contract Change Order is negotiated, this estimate will be updated.

Administrative Issues

5

Progress of Work

Administrative issues that remain on the SSD contract are related to setting project milestones and determining time related overhead resulting from the contract time extensions, escalation costs, and other necessary changes to the contract. Additionally, costs for implementing COZEEP for the East and West Tie-Ins need to be accounted for.

The following list of milestones has been provided to the Contractor to incorporate into the project schedule:

	Date	Status	Notes
W3L Complete	March 15th, 2007	Complete	finished 3/15/07
West Tie-In Phase 1 Viaduct Demo/Roll-In Complete	September 4th, 2007		
Access to W3R Available to CCM	January 2nd, 2008		
W3R, W4L/R, W6L/R, and W7L/R/Ramp Complete	December 31st, 2008		
Upper East Tie-In Area Available to CCM	April 2nd, 2009		
East Tie-In Roll-Out/Roll-In Complete	May 26th, 2009		
Frame 1 YBITS Area (Bent 7 West) Vacated by CCM	September 1st, 2009		
Project Completion	November 26th, 2009		

The Department has established a new completion date of November 26, 2009 and is negotiating for an equitable revised Time Related Overhead rate. Costs related to escalation and NOPC issues are also being negotiated with the Contractor. NOPC's with significant exposures include issues on the East Tie-In Design Criteria (NOPC #3, \$4.3M), Viaduct Segment Bearings Changes (NOPC #8, \$658K), and Design Submittal Review (NOPC #16, \$2.1M).

DRAFTStatus of Contract Change Orders: Administrative Issues

CCO	Method of Payment	Description	Plans from Design	CT Estimate Complete	CCM Estimate Complete	HQ ATP	TBPOC Approval	HQ I&A	Target TBPOC Meeting Date	CCO Executed	Anticipated CCO Cost
24S3		Contract Days Extention/TRO Compensation	N/A	No	No	N/A	Pending	Yes	TBD	No	\$11,781,000
56		Imbsen Claim Settlement	N/A	Yes	Yes	N/A	Pending	No	TBD	No	\$6,300,000
TBD		NOPC Closeout	N/A	No	No	N/A	Pending	No	TBD	No	\$5,000,000
TBD		Escalation Issues	N/A	No	No	N/A	Pending	No	TBD	No	\$5,000,000
N/A		Add Cozeep for WTI	N/A	No	N/A	N/A	N/A	N/A	N/A	N/A	\$1,000,000
N/A		Add COZEEP for ETI	N/A	No	N/A	N/A	N/A	N/A	N/A	N/A	\$1,000,000
Current Forecast for Adminstrative Issues											\$30,081,000

Bold = CCO's not issued yet

ATN = Authorization to Negotiate

ATP = Authorization to Proceed

The original contract allotment provided \$1.3M for COZEEP. However, with two full bridge closures planned additional funds will be required. The added COZEEP will not result in a Contract Change Order and is shown here to capture costs to the project.

Changes Since Last Report

No changes to report.

Budget Status

Costs of \$44.3M have been estimated for additional Time Related Overhead, escalation issues, and undefined risk items. As Contract Change Orders for these items are negotiated, the original estimate will be updated. Costs related to settlement of NOPC issues will be paid out of the contract contingency.

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BUDGET SUMMARY

Status of Changes on SSD Contract (June 2007):

	Scope of Work	Current Forecast
(0)	Original Bid Items + Previous CCO's	\$83.7
(1)	SSD New Viaduct	\$27.0
(2a)	West Tie-In Existing Viaduct Phase 1	\$34.8
(2b)	West Tie-In Phase 2	\$13.5
(3)	East Tie-In	\$36.9
(4)	YBI Transition Structures Advance Foundations	\$103.1
(5)	Administrative Issues	\$30.1
	Total	\$329.1

South South Detour (04-0120R4)

Construction Strategy (Scope)

Values:

- 1. Achieve Seismic Safety ASAP on Corridor
- 2. Foundation Risk-reduce schedule delays
- 3. Solve Constructability Issues Early
- 4. Minimize time traffic on SSD (Competing Value)
- 5. Cost

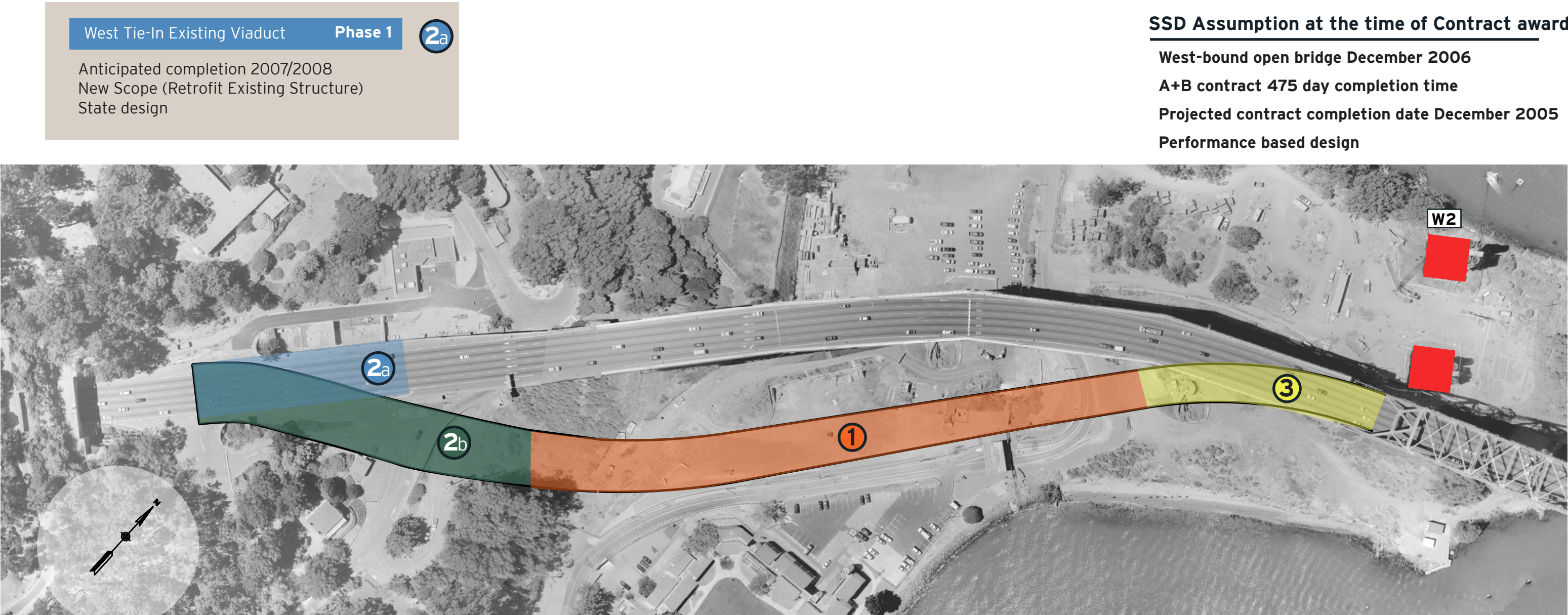
SSD Assumption at the time of Contract award

West-bound open bridge December 2006

A+B contract 475 day completion time

Projected contract completion date December 2005

Performance based design



NOT TO SCALE

West Tie-In **Phase 2**

2b

Anticipated completion 2008/2009
Existing scope modified
Performance based design (modified)

SSD New Viaduct

1

On-going, scheduled completion 2007
Original scope including CCO's 49 & 50
Performance based design

East Tie-In

3

Anticipated completion 2008/2009
No scope change - State design (T.Y. Lin)



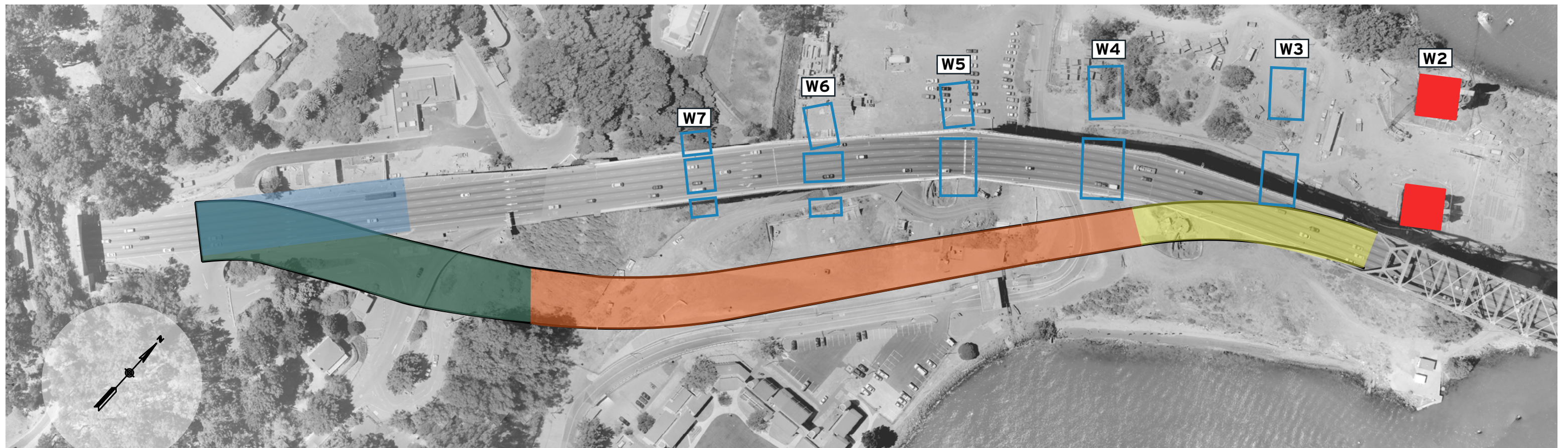
South South Detour (04-0120R4)

ATTACHMENT 1

YBITS Foundation by CCO

Yerba Buena Island Transition Structures Advance Foundations

Meets Values 1, 2 and 3
Work completed with bridge demolition



NOT TO SCALE



updated: 14 December 2006

Item 2: San Francisco-Oakland Bay Bridge
b. Yerba Buena Island
2) CCO's

**Item 2: San Francisco-Oakland Bay Bridge
c. West Approach CCO's**

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** June 21, 2007

FR: Tony Anziano, Caltrans Toll Bridge Program Manager

RE: Agenda No. - 2c
San Francisco-Oakland Bay Bridge
Item- West Approach CCO

Recommendation:

Approval to authorize Caltrans to negotiate Contract Change Order (CCO) No. 71, Supplement No. 1, West Approach, Delay Mitigation for West Piles 1L – 8L, not to exceed \$500,000. The original CCO No. 71 (\$6.7M) implemented the placement of a construction joint at the base of the permanent steel casing (PSC) for all remaining piles that were to be installed using an oscillated temporary casing. The effect on the time of completion of the contract was deferred on the change order pending completion of the work. This CCO Supplement is a delay mitigation measure.

Discussion:

Background and Analysis

In order to mitigate the risk of delay related to constructing piles, this change order shall provide an improved sequence of work concerning the removal of the latent or unsound concrete above the construction joint. This change order specifies that concrete be poured a minimum of 10 feet above the base of the PSC and that an auger and percussion drill be used to remove the top 8 feet of the concrete in order to establish a sound construction joint. This process is expected to considerably improve the progress of the work and limit the potential delays associated with it.

Specifically, the Contractor shall implement the following sequence of work concerning the placement of a construction joint above the base of the PSC of the pile as specified under the original CCO No. 71:

1. Pour concrete a minimum of 10 feet above the base of the pile's PSC.
2. Remove the concrete inside the pile's steel reinforcing cage to within 2 feet of the base of the PSC using an auger drill.

3. Break up the concrete between the pile's steel reinforcing cage and the PSC to within 2 feet of the base of the PSC using a percussion drill.
4. Hand chip and remove the remaining latent or unsound concrete in order to obtain a level construction joint of sound concrete.

This sequence of work shall apply to all remaining Stage 5 and Stage 6 piles associated with the original CCO No. 71.

The Contractor shall furnish an additional 8 temporary steel casings, used for retaining soil when the top of the PSC is below existing grade, in order to mitigate delays to the pile operations concerning CCO No. 71.

Schedule and Cost Status

This CCO Supplement is to mitigate potential project delay. The cost of delays to the project completion is estimated at approximately \$30,000 per day. This includes over \$15,000 per day in time-related overhead payments and an additional \$15,000 per day in extended falsework, k-rail and dedicated project equipment costs. Based on this information, a 2 to 4 month delay would result in costs of \$1.8M to \$3.6M.

The total estimated CCO cost of \$459,120.00 shall be financed from the contract's contingency fund.

Attachments:

- 1) Contract Change Order 71, Supplement 1, and Memorandum
- 2) CCO 71, S1 Issue and Approve Form
- 3) Executed Copy of CCO 71 and Memorandum

CONTRACT CHANGE ORDER

Change Requested by: Engineer

CCO 71 Suppl. No. 1 Contract No. 04 - 0435V4 Road SF-80-4.9/5.9

FED. AID LOC.:

To: TUTOR-SALIBA CORP

You are directed to make the following changes from the plans and specifications or do the following described work not included in the plans and specifications for this contract. **NOTE: This change order is not effective until approved by the Engineer.**

Description of work to be done, estimate of quantities and prices to be paid. (Segregate between additional work at contract price, agreed price and force account.) Unless otherwise stated, rates for rental of equipment cover only such time as equipment is actually used and no allowance will be made for idle time. This last percentage shown is the net accumulated increase or decrease from the original quantity in the Engineer's Estimate.

The Contractor shall implement the following sequence of work concerning the placement of a construction joint above the base of the permanent steel casing (PSC) of the pile as specified under the original Change Order No. 71:

- 1) Pour concrete a minimum of 10 feet above the base of the pile's PSC.
- 2) Remove the concrete inside the pile's steel reinforcing cage to within 2 feet of the base of the PSC using an auger drill.
- 3) Break up the concrete between the pile's steel reinforcing cage and the PSC to within 2 feet of the base of the PSC using a percussion drill.
- 4) Hand chip and remove the remaining latent or unsound concrete in order to obtain a level construction joint of sound concrete.

This sequence of work shall apply to all remaining Stage 5 and Stage 6 piles associated with the original Change Order No. 71.

The Contract shall furnish and additional 8 temporary steel casings, used for retaining soil when the top of the PSC is below existing grade, in order to mitigate delays to the pile operations concerning Change Order No. 71.

Extra Work at Force Account:

In accordance with Section 9-1.03 of the Standard Specification, compensate the Contractor for the mobilization, operating time and idle time concerning the auger and percussion drill rigs performing the work as defined by this change. Compensation for labor shall be paid as it pertains to the operation of the drill rigs only. All other labor costs shall be considered to be included in the agreed lump sum price as specified below.

Estimated cost of Extra Work at Force Account\$200,000.00

Adjustment of Compensation at Unit Price:

Work extended crew hours, as determined by the Engineer, concerning the hand chipping and removal of latent concrete in order to obtain a level construction joint of sound concrete.

For each crew hour of overtime worked above 40 hours per week, the Contractor shall be compensated an agreed unit price of \$165.00 per hour. A crew shall be defined as 7 laborers and 1 operator working on two piles simultaneously. This unit price constitutes full compensation for all costs, including markups, associated with this change.

Estimated Cost of Adjustment of Compensation at Agreed Unit Price 250 Hours @ \$165.00 / Hour = \$41,250.00

Adjustment of Compensation at Lump Sum:

Provide compensation to the Contractor for all additional costs concerning furnishing, placing, removing and disposing of the 8 foot concrete over pour of the pile construction joint as defined above.

Provide compensation to the Contractor for furnishing the additional 8 temporary casings as defined above.

For this work, the Contractor shall be compensated an agreed lump sum of \$217,870.00. This lump sum constitutes full compensation for all costs, including markups, associated with this change.

Cost of Adjustment of Compensation at Agreed Lump Sum\$217,870.00

CONTRACT CHANGE ORDER

Change Requested by: Engineer

CCO 71	Suppl. No. 1	Contract No. 04 - 0435V4	Road SF-80-4.9/5.9	FED. AID LOC.:
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Should Contractor-Controlled Insurance Program costs apply, these costs will be determined separately and compensated by the Department.

Estimated Cost: Increase ☒ Decrease ☐ \$459,120.00

By reason of this order the time of completion will be adjusted as follows: Deferred

Submitted by

Signature <i>Deanna Vilcheck</i>	Resident Engineer Deanna Vilcheck	Date 6/13/07
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Approval Recommended by

Signature <i>Mike Forner</i>	Principal Construction Manager Mike Forner	Date 6/20/07
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Engineer Approval by

Signature	Principal Construction Manager Mike Forner	Date
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We the undersigned contractor, have given careful consideration to the change proposed and agree, if this proposal is approved, that we will provide all equipment, furnish the materials, except as may otherwise be noted above, and perform all services necessary for the work above specified, and will accept as full payment therefor the prices shown above.

NOTE: If you, the contractor, do not sign acceptance of this order, your attention is directed to the requirements of the specifications as to proceeding with the ordered work and filing a written protest within the time therein specified.

Contractor Acceptance by

Signature	(Print name and title)	Date
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CONTRACT CHANGE ORDER MEMORANDUM

DATE: 5/15/2007 Page 1 of 2

TO: Dennis Turchon / Deanna Vilcheck		FILE: E.A. 04 - 0435V4	
FROM: Deanna Vilcheck		CO-RTE-PM SF-80-4.9/5.9	
FED. NO.			
CCO#: 71	SUPPLEMENT#: 1	Category Code: BZZZ	CONTINGENCY BALANCE (incl. this change) \$6,066,415.69
COST: \$459,120.00	INCREASE <input checked="" type="checkbox"/> DECREASE <input type="checkbox"/>	HEADQUARTERS APPROVAL REQUIRED? <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO	
SUPPLEMENTAL FUNDS PROVIDED: \$0.00		IS THIS REQUEST IN ACCORDANCE WITH ENVIRONMENTAL DOCUMENTS? <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO	
CCO DESCRIPTION: Delay Mitigation for Wet Piles 1L - 8L		PROJECT DESCRIPTION: SEISMIC RETROFIT	
Original Contract Time: 1824 Day(s)	Time Adj. This Change: DEF Day(s)	Previously Approved CCO Time Adjustments: 52 Day(s)	Percentage Time Adjusted: (including this change) 3 %
			Total # of Unreconciled Deferred Time CCO(s): (including this change) 0

THIS CHANGE ORDER PROVIDES FOR:

The mitigation of anticipated Department delays to the contract completion concerning the work specified under the original Change Order No. 71.

The original Change Order No. 71 implemented the placement of a construction joint at the base of the permanent steel casing (PSC) for all remaining piles that were to be installed using an oscillated temporary casing. The placement of the construction joint required an over pour of concrete and the removal of the unsound, latent or contaminated concrete in order to establish a construction joint with sound concrete. The affect on the time of completion of the contract was deferred on the change order pending completion of the work.

The contractor currently over pours the base of the permanent steel casing by a minimum of 7 foot and then removes the latent or unsound concrete by hand chipping. Numerous iterations of concrete removal, based on Department direction, are typically required before a sound concrete construction joint is obtained. While this process fulfills the contractual requirements of the original Change Order No. 71, the time it takes to complete the iterations has been identified as the major cause of delay to the work.

The delay has been incurred due to the time period between the initial concrete pour, to establish the construction joint, and when the second concrete pour takes place to complete the pile's construction. This process has resulted in anywhere from 2 weeks to 3 months of delay in the construction of each pile.

During Stage 2 pile construction, the Department realized 2 months of project delay in constructing the 17 piles associated with this work. During Stage 3 construction, the Department realized 3 months of delay for the 12 piles constructed. There are 21 piles remaining on the project that require the placement of a construction joint all of which currently are on the critical path of the contract's completion. It is anticipated, based on previous results, that a project delay of 2 to 4 months will be incurred in the construction of these piles and this potential delay has been identified as the major risk associated within the project's risk management analysis.

The cost of delays to the project completion is estimated at approximately \$30,000 per day. This includes over \$15,000 per day in time related overhead payments and an additional \$15,000 per day in extended falsework, k-rail and dedicated project equipment costs. Based on this, a 2 to 4 month delay would result in costs of \$1,800,000 to \$3,600,000.

In order to mitigate this risk, this change order shall provide an improved sequence of work concerning the removal of the latent or unsound concrete above the construction joint. This change order specifies that concrete be poured a minimum of 10 feet above the base of the PSC and that an auger and percussion drill be used to remove the top 8 feet of the concrete in order to establish a sound construction joint. This process is expected to considerably improve the progress of the work and limit the potential delays associated with it.

Costs associated with the implementation of the revised work sequencing shall include the mobilization and use of the auger and percussion drill rigs for approximately 3 months and furnishing, placing, removing and disposing of additional concrete. In order to further mitigate delays the change order shall provide for extended crew hours to be worked concerning the removal of the concrete over pour.

The change order also provides compensation for the contractor to furnish an additional 8 temporary steel casing used to retain soil above the top of the PSC when it is below the existing grade. These additional casings will mitigate potential

CONTRACT CHANGE ORDER MEMORANDUM

EA: 0435V4 CCO: 71 - 1

DATE: 5/15/2007

Page 2 of 2

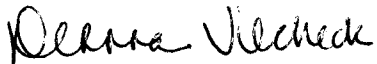
Department delays by preventing the idling of the pile drill rig and crew due to the existing temporary casings being employed at the piles during the concrete removal operations.

Compensation for furnishing, placing, removing and disposing of the concrete over pour shall be paid as an adjustment of compensation at an agreed lump sum \$217,870.00. Compensation for any extended hours worked, as determined by the Engineer, concerning the concrete chipping crew shall be paid as an adjustment of compensation at an agreed unit price of \$165.00 per crew hour at an estimated cost of \$41,250.00. Compensation for mobilization and operating and idle time associated with the percussion and auger drill rigs shall be paid as extra work at force account at an estimated cost of \$200,000.00. The total estimated change order cost of \$459,120.00 shall be financed from the contract's contingency fund. A cost analysis is on file.

Adjustment of contract time is deferred in concert with the original change order as the work will affect the controlling operation.

Maintenance concurrence is not required as the change doesn't affect any permanent roadway features.

This change order received authority to proceed with the work on June 1, 2007 from the Division of Construction at an amount not to exceed \$500,000.00.

CONCURRED BY:			ESTIMATE OF COST		
Construction Engineer:	D. Vilcheck	Date	6/13/07	THIS REQUEST	TOTAL TO DATE
Bridge Engineer:	Hazzaa El-Mahmoud	Date		ITEMS	\$0.00
Project Engineer:	H. Wong	Date		FORCE ACCOUNT	\$200,000.00
Project Manager:	A. Melkonians	Date		AGREED PRICE	\$0.00
FHWA Rep.:		Date		ADJUSTMENT	\$259,120.00
Environmental:		Date		TOTAL	\$459,120.00
Other (specify):	HQ ATP by Ken Darby	Date	6/1 /07	FEDERAL PARTICIPATION	
Other (specify):		Date		<input type="checkbox"/> PARTICIPATING <input type="checkbox"/> PARTICIPATING IN PART <input checked="" type="checkbox"/> NONE <input type="checkbox"/> NON-PARTICIPATING (MAINTENANCE) <input type="checkbox"/> NON-PARTICIPATING	
District Prior Approval By:		Date		FEDERAL SEGREGATION (if more than one Funding Source or P.I.P. type)	
HQ (Issue /Approve) By:		Date		<input type="checkbox"/> CCO FUNDED PER CONTRACT <input type="checkbox"/> CCO FUNDED AS FOLLOWS	
Resident Engineer's Signature:		Date		FEDERAL FUNDING SOURCE	PERCENT
			6/13/07		



California Department of Transportation

CONSTRUCTION DIVISION

TO: District 4 CCO Desk

Date: 6/1/2007

Contract No.: 4 - 0435V4

Road: SF-80-4.9/5.9

FED. No.:

To: SARTIPI - 04

Attention: 04 - SARWARY

HQ Direction:

TO PROCEED WITH THE WORK

CCO No. 071

Sup. No. 1

Rev. No. 0

Per Your Submittal Dated: 6/1/2007

CCO Category Code: F - H - C - A

PROVIDES A MODIFIED CONSTRUCTION METHOD FOR PLACING CIDH PILES IN PERMANENT STEEL CASINGS AND ESTABLISHMENT OF A SOUND CONSTRUCTION JOINT FOR THE REMAINING 21 PILES TO AVOID POTENTIAL STATE-CAUSED DELAYS.

RECOMMENDED AUTHORIZATION TO PROCEED IS CONDITONAL ON THE FOLLOWING:

1. OBTAINING THE TBPOC'S AUTHORIZATION TO PROCEED WITH THIS CHANGE SINCE THE CCO'S CUMMULATIVE COST EXCEEDS \$1,000,000.00.
2. THE SUBMITTAL OF A COMPLETE I&A REQUEST PACKAGE TO DIVISION OF CONSTRUCTION BY JULY 1, 2007.

THE TOTAL COST OF THIS CHANGE, INCLUDING THIS SUPPLEMENTAL, IS SHOWN AS \$7,200,000.00 WITH A DEFERRED TIME ADJUSTMENT.

Items:	\$0.00
Force Account:	\$500,000.00
Agreed Price:	\$0.00
Adj. of Comp.	\$0.00

Total: \$500,000.00

Time: (DEFERRED)

ELIZABETH DOOHER,
Acting Assistant Division
Chief, by:

Ken Darby

Division of Construction
1120 "N" Street, MS-44, Sacramento, CA 95814
Fax Number: (916) 654-5735
To Confirm Transmission, Call (916) 654-5259



California Department of Transportation

CONSTRUCTION DIVISION

TO: District 4 CCO Desk

Date: 6/21/2007

Contract No.: 4 - 0435V4

Road: SF-80-4.9/5.9

FED. No.:

To: SARTIPI - 04

Attention: 04 - SARWARY

HQ Direction:

TO ISSUE AND APPROVE

CCO No. 071

Sup. No. 1

Rev. No. 1

Per Your Submittal Dated: 6/21/2007

CCO Category Code: F - H - C - A

PROVIDES A MODIFIED CONSTRUCTION METHOD FOR PLACING CIDH PILES IN PERMANENT STEEL CASINGS AND ESTABLISHMENT OF A SOUND CONSTRUCTION JOINT FOR THE REMAINING PILES IN STAGES 5 AND 6 TO AVOID POTENTIAL STATE-CAUSED DELAYS.

RECOMMENDED ISSUE AND APPROVE IS CONDITONAL ON THE FOLLOWING:

1. OBTAINING THE TBPOC'S APPROVAL OF THIS CHANGE SINCE THE CCO'S CUMMULATIVE COST EXCEEDS \$1,000,000.00.
2. WITHIN THE CCO MEMO, SHOW THE CONCURRENCE DATES FOR THE PROJECT ENGINEER AND PROJECT MANAGER.
3. WITHIN THE CCO MEMO, REVISE THE CCO CODING TO THAT OF THE ORIGINAL CCO.
4. OBTAINING THE CONTRACTOR'S SIGNATURE ON THE AGREED PRICE CCO. NOTE THAT FAILURE TO OBTAIN THE CONTRACTOR'S SIGNATURE WILL REQUIRE A REVISED CCO AND NEW I&A REQUEST.
5. THE TIMELY PROCESSING OF A SUPPLEMENTAL CCO TO RESOLVE THE DEFERRED TIME OF THIS CHANGE.

THE TOTAL COST OF THIS CHANGE, INCLUDING THIS SUPPLEMENTAL, IS SHOWN AS \$7,159,120.00 WITH A DEFERRED TIME ADJUSTMENT.

Items:	\$0.00
Force Account:	\$200,000.00
Agreed Price:	\$0.00
Adj. of Comp.	\$259,120.00

Total: **\$459,120.00**

ELIZABETH DOOHER,
Acting Assistant Division
Chief, by:

Time: **(DEFERRED)**

Ken Darby

Date: 6/21/2007

Page 2 of 2

Contract No.: 4 - 0435V4

Road: SF-80-4.9/5.9

FED. NO.:

CCO No. 071 **Sup. No.** **1 Rev. No.** 1 **CCO Category Code:** F - H - C - A

Continued:

Division of Construction
1120 "N" Street, MS-44, Sacramento, CA 95814
Fax Number: (916) 654-5735
To Confirm Transmission, Call (916) 654-5259

CONTRACT CHANGE ORDER

Change Requested by: Engineer

CCO: 71	Suppl. No. 0	Contract No. 04 - 0435V4	Road SF-80-4.9/5.9	FED. AID LOC.:
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To: TUTOR-SALIBA CORP

You are directed to make the following changes from the plans and specifications or do the following described work not included in the plans and specifications for this contract. **NOTE: This change order is not effective until approved by the Engineer.**

Description of work to be done, estimate of quantities and prices to be paid. (Segregate between additional work at contract price, agreed price and force account.) Unless otherwise stated, rates for rental of equipment cover only such time as equipment is actually used and no allowance will be made for idle time. This last percentage shown is the net accumulated increase or decrease from the original quantity in the Engineer's Estimate.

Extra Work at Lump Sum:**1. Extra Work at Agreed Lump Sum:**

Mitigate pile anomalies in the area of the permanent steel casing (PSC) in close proximity to the bottom of the column rebar cage. Specifically this change compensates for the mitigation of anomalies in the PSC in piles 6U1, 5U2, 5U1, 4U2, 4U1, 3U2, 3U1, 2U1 and 2U2. Exclusive of these piles, mitigation of all anomalies in piles within the PSC, shall be the responsibility of the Contractor.

For this work, the Contractor will be paid the agreed lump sum of \$2,360,000. This sum constitutes full compensation for providing all labor, materials, equipment, tools and incidentals, including all markups, for the work of this change. The amount paid includes all direct and indirect costs, costs associated with idle time on equipment, including but not limited to potential inefficiencies, escalations in labor and material costs, changes in equipment rental rates, time related overhead, overhead and other costs due to delays to the Contractor's schedule. No additional compensation will be provided for pile anomaly mitigation in all piles constructed to date.

Extra Work at Agreed Lump Sum \$2,360,000.

Adjustment of Compensation at Lump Sum:**2. Adjustment of Compensation at Agreed Lump Sum:**

Premium time costs to complete the remaining work on Frames 1U through 6U (Stage 1) and on Frame 3U (Stage 2).

For this work, the Contractor will be paid the agreed lump sum of \$100,000. This sum constitutes full compensation for providing all labor, materials, equipment, tools and incidentals, including all markups, for the work of this change. The amount paid includes all direct and indirect costs, costs associated with idle time on equipment, including but not limited to potential inefficiencies, escalations in labor and material costs, changes in equipment rental rates, time related overhead, overhead and other costs due to delays to the Contractor's schedule. No additional compensation will be provided for delays relating to pile anomaly mitigation in all piles constructed to date.

Adjustment of Compensation at Agreed Lump Sum: \$100,000

3. Adjustment of Compensation at Agreed Lump Sum:

Place a construction joint above the base of the PSC, at an elevation sufficient to create a seal to prevent water and deleterious material from entering the PSC, for the 47 remaining Mainline piles and the 12 remaining 5th Street piles that will be installed using oscillated temporary casings. These remaining piles include 10 piles on the SFFOBLO Line at Bents 1L and 2L; 27 piles on the SFFOBLO Line at Bents 3L, 4L, 5L, 6L, 7L and 8L; 6 piles on the SFFOBUP Line at Bents 1U and 2U; 4 piles on the SFFOBUP Line at Bents 3U and 4U; 6 piles on the 5th Street On Ramp at Bents 6, 7 and 8; and 6 piles on the 5th Street Off Ramp at Bents 6, 7 and 8. This lump sum cost includes but is not limited to, remobilization and all equipment necessary to dewater, remove unsound, latent and contaminated concrete above the construction joint elevation, support the PSC and insure proper location of the PSC.

CONTRACT CHANGE ORDER

Change Requested by: Engineer

CCO: 71	Suppl. No. 0	Contract No. 04 - 0435V4	Road SF-80-4.9/5.9	FED. AID LOC.:
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Permitting the use of a construction joint in the PSC, shall relieve the Department of responsibility for any pile anomalies in the PSC portion of the pile. The Contractor shall be responsible for all costs that are the result of potential differing site conditions in all constructed piles and future piles within the PSC. The Contractor shall also be responsible for all costs that are the result of all potential groundwater differing site conditions in all constructed piles and future piles with a PSC.

For this work, the Contractor will be paid the agreed lump sum of \$4,240,000. This sum constitutes full compensation for providing all labor, materials, equipment, tools and incidentals, including all markups, for the work of this change. The amount paid includes all direct and indirect costs, costs associated with idle time on equipment, including but not limited to potential inefficiencies, escalations in labor and material costs and changes in equipment rental.

Consideration of a time adjustment for the work described in Item No. 3 of this change will be deferred until completion of the work specified in this item of the contract change order. A determination of a time extension will be in accordance with Section 8-1.07, "Liquidated Damages," of the Standard Specifications.

Cost of Adjustment of Compensation at Agreed Lump Sum: \$4,240,000

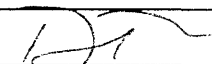
The Contractor agrees to accept this Contract Change Order as full resolution of NOPC No. 1 – CIDH Piles – Change from 3,500 psi to 4,000 psi compressive strength concrete, NOPC No. 8 – Acceptance (Gamma Gamma) Testing, and NOPC No. 11 – CIDH Piles – Addition of PVC Acceptance Tubes. The Contractor agrees to accept this Contract Change Order as full resolution for all direct and indirect costs, excluding time related overhead costs for NOPC No. 13 – Pile Anomalies.

Should Contractor-Controlled Insurance Program cost apply, these costs will be determined separately and compensated for by the Department.

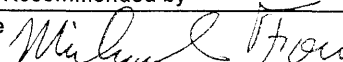
Estimated Cost: Increase ☒ Decrease ☐ \$6,700,000.00

By reason of this order the time of completion will be adjusted as follows: Deferred

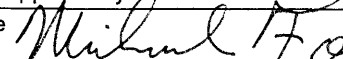
Submitted by

Signature 	Resident Engineer: Deanna Vilcheck	Date: 9/27/15
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Approval Recommended by

Signature 	Construction Engineer: Dennis Turchon Mike Fourn	Date: 9-27-05
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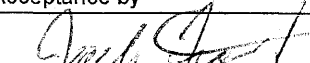
Engineer Approval by

Signature 	(Print name and title) Dennis Turchon - Chief	Date
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We the undersigned contractor, have given careful consideration to the change proposed and agree, if this proposal is approved, that we will provide all equipment, furnish the materials, except as may otherwise be noted above, and perform all services necessary for the work above specified, and will accept as full payment therefor the prices shown above.

NOTE: If you, the contractor, do not sign acceptance of this order, your attention is directed to the requirements of the specifications as to proceeding with the ordered work and filing a written protest within the time therein specified.

Contractor Acceptance by

Signature 	(Print name and title) Jack Frost Sr. V.P.	Date: 9/27/05
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CONTRACT CHANGE ORDER MEMORANDUM

DATE: 5/18/2005 Page 1 of 2

TO: Dennis Turchon / Dennis Turchon			FILE: E.A. 04 - 0435V4
FROM: Deanna Vilcheck			CO-RTE-PM SF-80-4.9/5.9
FED. NO.			
CCO#: 71	SUPPLEMENT#: 0	Category Code: FHCA	CONTINGENCY BALANCE (incl. this change) \$3,300,161.42
COST: \$6,700,000.00 INCREASE <input checked="" type="checkbox"/> DECREASE <input type="checkbox"/>			HEADQUARTERS APPROVAL REQUIRED? <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO
SUPPLEMENTAL FUNDS PROVIDED: \$0.00			IS THIS REQUEST IN ACCORDANCE WITH ENVIRONMENTAL DOCUMENTS? <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO
CCO DESCRIPTION: NOPC13 & Construction Joint in CIDH Piles			PROJECT DESCRIPTION: SEISMIC RETROFIT
LOCATION: IN THE CITY AND COUNTY OF SAN FRANCISCO FROM FIFTH STREET TO SAN FRANCISCO-OAKLAND BAY BRIDGE			

THIS CHANGE ORDER PROVIDES FOR:

Mitigation of pile anomalies in the permanent steel casing (PSC), premium time and any additional work necessary to complete remaining work on Frames 1U through 6U and for placing a construction joint at the base of the PSC when piles are installed using oscillated temporary casings.

On February 1, 2005 the Dispute Review Board (DRB) made a unanimous recommendation for the resolution of Notice of Potential Claim (NOPC) No. 13 and two previous NOPC's that were incorporated into this dispute, namely, NOPC No. 1 – CIDH Piles – Change from 3,500 psi to 4,000 psi compressive strength concrete and NOPC No. 11 – CIDH Piles – Addition of PVC Acceptance Tubes.

Section 10-1.60 "Piling" of the Special Provisions specified slurry and or temporary steel casings to support the pile excavation below the bottom of the PSC. The presence of "quick soil" on the Mainline precluded the use of slurry alone. The DRB stated that there was an inherent constructability issue in the contract documents that required the column cage to be set prior to concrete placement without a construction joint. The DRB concluded that the majority of the anomalies in the Mainline piles occurred in close proximity to the bottom of the rebar cage where the Contractor was required to suspend its concrete placing operations in order to set the column rebar cage. The DRB believes that the pause in concrete placing operations and that the withdrawal and reinsertion of the concrete tremie pipe were contributors to the anomalies near the bottom of the column rebar cage. As a consequence, the DRB believes the repair of anomalies in close proximity to the bottom of the pile rebar cage are the responsibility of the Department.

Based on the DRB ruling, the Department shall compensate the Contractor for mitigation of anomalies in the PSC. Specifically this change compensates for the mitigation of anomalies in the PSC, in piles 6U1, 5U2, 5U1, 4U2, 4U1, 3U2, 3U1, 2U1 and 2U2. Exclusive of these piles, mitigation of all anomalies in piles that have a PSC shall be the responsibility of the Contractor. No additional compensation will be provided for delays relating to any anomaly mitigation in piles with a PSC. No adjustment in contract time will be made due to the mitigation of anomalies in the PSC since this work is not a controlling item.

Frames 1U, 2U and 3U contain anomalies that were the responsibility of the Department. To minimize the impacts the anomaly mitigation in these Frames have on the completion date of the Contract, the Department will direct the Contractor to provide additional work forces, work extended hours, provide material and equipment to mitigate potential delays for the completion of Frames 1U through 6U. No adjustment in contract time will be made due to the mitigation of potential delays for the completion of Frames 1U through 6U.

The Department shall compensate the Contractor for the additional work associated with placing the construction joint above the base of the PSC, including but not limited to mobilization, removing unsound concrete under confined space conditions and supporting the reinforcing cage. Consideration of a time adjustment will be deferred until completion all piles with a PSC. Determination of a commensurate time extension will be made in accordance with Section 8-1.07, "Liquidated Damages," of the Standard Specifications.

The Contractor agrees to accept this Contract Change Order as full resolution of NOPC No. 1 – CIDH Piles – Change from 3,500 psi to 4,000 psi compressive strength concrete, NOPC No. 8 – Acceptance (Gamma Gamma) Testing, NOPC No. 11 – CIDH Piles – Addition of PVC Acceptance Tubes. The Contractor agrees to accept this Contract Change Order as full resolution of NOPC No. 13 – Pile Anomalies, exclusive of time related overhead.

The Contractor will be paid an agreed sum of \$6,700,000 for this change. The work can be financed from the contingency funds. A force account analysis is on file in the project records.

CONTRACT CHANGE ORDER MEMORANDUM

EA: 0435V4 CCO: 71 - 0

DATE: 5/18/2005

Page 2 of 2

John Drury of OSC, Gudmund Setberg of OSD and Project Manager Ken Terpstra have concurred with this change.

Maintenance concurrence is not required for this work.

No adjustment of contract time is warranted for Item Nos. 1 and 2 of this change, as these items do not affect the controlling operation. Consideration of a time adjustment for Item No. 3 of this change will be deferred until completion of the work specified in Item No. 3 of this contract change order. A determination of a time extension for Item No. 3 will be in accordance with Section 8-1.07, "Liquidated Damages," of the Standard Specifications.

CONCURRED BY:		ESTIMATE OF COST	
Construction Engineer: D. Vilcheck <i>DMT</i>	Date <i>9/22/05</i>	THIS REQUEST	TOTAL TO DATE
Bridge Engineer: W. Al-Basha <i>MS</i>	Date <i>9/29/05</i>	ITEMS \$0.00	\$0.00
FHWA Representative:	Date	FORCE ACCOUNT \$0.00	\$0.00
Project Engineer:	Date	AGREED PRICE \$2,360,000.00	\$2,360,000.00
Other (specify):	Date 8/8/2005	ADJUSTMENT \$4,340,000.00	\$4,340,000.00
Ken Terpstra-Project Manager		TOTAL \$6,700,000.00	\$6,700,000.00
		FEDERAL PARTICIPATION	
Gudmund Setberg (OSD)	Date 7/25/2005	<input type="checkbox"/> PARTICIPATING <input type="checkbox"/> PARTICIPATING IN PART <input checked="" type="checkbox"/> NONE <input type="checkbox"/> NON-PARTICIPATING (MAINTENANCE) <input type="checkbox"/> NON-PARTICIPATING	
John Drury (OSC) <i>TEPOL 9/22/05</i>	Date 4/19/2005	FEDERAL SEGREGATION (if more than one Funding Source or P.I.P. type)	
District Prior Approval By:	Date	<input type="checkbox"/> CCO FUNDED PER CONTRACT <input type="checkbox"/> CCO FUNDED AS FOLLOWS	
HQ (Issue Approve) By: <i>Ken Darby</i>	Date <i>9/27/05</i>	FEDERAL FUNDING SOURCE	PERCENT
Resident Engineer's Signature: <i>Deanna Vilcheck</i>	Date <i>9/28/05</i>		

MIKE FORNER

9-28-05

**Item 2: San Francisco-Oakland Bay Bridge
d. Skyway CCO**

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** June 21, 2007

FR: Tony Anziano, Caltrans Toll Bridge Program Manager

RE: Agenda No. - 2d
San Francisco-Oakland Bay Bridge
Item- Skyway CCO

Recommendation:

Approval to authorize Caltrans to execute Contract Change Order (CCO) No. 104, Supplement No. 1, Skyway Painting, for the amount of \$1,867,645.00. CCO 104 will allow for the painting of galvanized or otherwise coated steel components of the Skyway Bridge.

Discussion:

Background

CCO 104 S 0 was executed on 5/13/05 for the amount of \$795,059.00. CCO 104 S 0 revised the primer specification and changed the finish paint from latex to polysiloxane top coats to match SAS paint specification.

In mid-2006, an architectural review of the project and contract documents revealed that some visible steel surfaces, which are supposed to be painted white, were shown to be galvanized or otherwise coated per contract specifications and plans. This CCO is needed to paint, with white paint, the visible galvanized or otherwise coated steel components to meet the design intent.

Analysis

As authorized by the Engineer, and in addition to areas specified to be painted elsewhere in the Special Provisions, paint the following visible areas in accordance with the provisions of CCO 104, of the SFOBB East Span Skyway project:

1. The galvanized traveler support rails on the bikepath and orthotropic box girders.
2. The bikepath handrail, attachment pieces, light fixtures, and emergency gates except for the emergency gate wheels.
3. The PVC coated conduit on the bikepath.

4. The belvedere benches on the bikepath belvederes.
5. The service platforms access stairs and railing.
6. The service platforms handrails and C200 support channel face.
7. The cabinets, conduits, and support legs on the service platforms, except for any stainless steel components, rubber gaskets, nameplates, and sealflex conduit.
8. The overhead and CMS sign support structures.
9. The 6, 10, and 12 NPS ductile iron pipe, on the outside of the orthotropic box girders, including the transition, fittings, supports and hangers.
10. The substation access stairs and railings from the barrier rail to and inclusive of the first landing, except for the bottom of the stairs and the bottom of the landing surface.

All finish coats are to be applied after installation of the components to be painted.

Schedule and Cost Status

There will be no schedule impacts.

For the extra work associated with this change order, as provided in the revised specifications, the Contractor agrees to accept an agreed lump sum amount of \$1,867,645.00. This agreed lump sum constitutes full compensation, including markups, for all direct and indirect costs, all associated project impacts including environmental and weather impacts, and all overhead costs.

The Budget Balance Beam for the Skyway Contract 04-012024 is also attached. As of June 5, 2007, there is a projected overall cost savings of \$48M.

Attachments:

- 1) Contract Change Order 104 S 1
- 2) Contract Change Order 104 S 1 Memorandum
- 3) Skyway Contract 04-012024 Budget Analysis, June 5, 2007
- 4) Record of Prior Approvals

CONTRACT CHANGE ORDER

Change Requested by: Engineer

CCO: 104	Suppl. No. 1	Contract No. 04 - 012024	Road 04-SF,Ala-80- 13.9/14.3,0.0/1.6	FED. AID LOC.: ACIM-080-1(085)8N
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To: KIEWIT / FCI / MANSON a JV

You are directed to make the following changes from the plans and specifications or do the following described work not included in the plans and specifications for this contract. **NOTE: This change order is not effective until approved by the Engineer.**

Description of work to be done, estimate of quantities and prices to be paid. (Segregate between additional work at contract price, agreed price and force account.) Unless otherwise stated, rates for rental of equipment cover only such time as equipment is actually used and no allowance will be made for idle time. This last percentage shown is the net accumulated increase or decrease from the original

Revise the Special Provisions for the SFOBB East Span Skyway Bridge Replacement Project, Bridge Nos. 340006L/R as follows:

- 1.) Add the following sentence to Section 10-1.48, Traveler Support Rails, as paragraph six: "Traveler rails shall receive a finish paint coat in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."
- 2.) Add the following "Painting" sub-section to Section 10-1.51, Sign Structures: "Overhead sign structures shall be galvanized and painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."
- 3.) Revise the fifth paragraph of Sub-section "Steel pipe railings and hand railings" under Section 10-1.57, Miscellaneous Metal (Substation, Service Platforms, and Walkways), as follows: " Steel railings shall be galvanized and painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions. After galvanizing, all elements of the railing shall be free of fins, abrasions, rough or sharp edges, and other surface defects and shall not be kinked, twisted or bent."
- 4.) Add the following "Painting" sub-section to Section 10-1.57, Miscellaneous Metal (Substation, Service Platforms, and Walkways), after Sub-section, Steel pipe railings and hand railings; "Miscellaneous Metal in the Substations, Service Platforms, and Walkways designated to be painted shall be painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."
- 5.) Revise sub-section "Painting" of Section 10-1.58, Hinged Doors (Substations), as follows: " Except for the primer application specified herein, doors and frames shall be cleaned, prepared and painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."
- 6.) Add the following sentence to Section 10-1.62, Metal Bridge Railing, as paragraph two: " All galvanized bikepath railing elements shall be painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."
- 7.) Add the following sentence to Section 10-3.05, Conduit, Sub-section, Polyvinyl Chloride Coated Galvanized Rigid Steel Conduit, Threaded Couplings, and Elbows, as paragraph two: "The polyvinyl chloride (PVC) coated galvanized rigid steel (GRS) conduit, threaded couplings, and elbows designated to be painted shall be free from moisture, visible dust, visible grease, or other deleterious materials immediately prior to application of the finish coat in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."
- 8.) Revise paragraph five of Sub-section "Pipe and fittings for line 4 NPS or greater" of Section 10-4.04, CCSF Water Main and Sewage Force Main, as follows: "Ductile iron pipe and fittings shall have cement mortar lining and seal coating conforming to ANSI/AWWA C104/A21.4. Pipe shall have a bituminous outside coating conforming to ANSI/AWWA C151/A21.51 and fittings shall have a bituminous outside coating conforming to ANSI/AWWA C110/A21.10, except for the pipe and fittings on the outside of the orthotropic transition span, which shall be primed with "MC-FerroClad" TM or equal and painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."

CONTRACT CHANGE ORDER

Change Requested by: Engineer

CCO: 104	Suppl. No. 1	Contract No. 04 - 012024	Road 04-SF,Ala-80- 13.9/14.3,0.0/1.6	FED. AID LOC.: ACIM-080-1(085)8N
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Extra Work at Lump Sum:

As authorized by the Engineer, and in addition to areas specified to be painted elsewhere in the Special Provisions, paint the following visible areas in accordance with the provisions of Contract Change Order No. 104 Supplement 0, of the SFOBB East Span Skyway project, Bridge Nos. 34-0006L/R:

- 1.) The galvanized traveler support rails on the bikepath and orthotropic box girders.
- 2.) The bikepath handrail, attachment pieces, light fixtures, and emergency gates except for the emergency gate wheels.
- 3.) The PVC coated conduit on the bikepath.
- 4.) The belvedere benches on the bikepath belvederes.
- 5.) The service platforms access stairs and railing.
- 6.) The service platforms handrails and C200 support channel face.
- 7.) The cabinets, conduits, and support legs on the service platforms, except for any stainless steel components, rubber gaskets, nameplates, and sealflex conduit.
- 8.) The overhead and CMS sign support structures.
- 9.) The 6, 10, and 12 NPS ductile iron pipe, on the outside of the orthotropic box girders, including the transition, fittings, supports and hangers.
- 10.) The substation access stairs and railings from the barrier rail to and inclusive of the first landing, except for the bottom of the stairs and the bottom of the landing surface.

All finish coats are to be applied after installation of the components to be painted.

For the extra work associated with this change order, as provided in the revised specifications, the Contractor agrees to accept an agreed lump sum amount of \$1,867,645.00. This agreed lump sum constitutes full compensation, including markups, for all direct and indirect costs, all associated project impacts including environmental and weather impacts, and all overhead costs.

Extra Work at Agreed Lump Sum\$1,867,645.00

Estimated Cost: Increase ☒ Decrease ☐ \$1,867,645.00

By reason of this order the time of completion will be adjusted as follows: 0 days

Submitted by

Signature	Resident Engineer DOUG COE, Supervising Br. Eng.	Date 6-1-07
-----------	---	----------------

Approval Recommended by

Signature	Construction Engineer PETER SIEGENTHALER	Date 6-1-07
-----------	---	----------------

Engineer Approval by

Signature	Construction Manager PETER SIEGENTHALER	Date
-----------	--	------

We the undersigned contractor, have given careful consideration to the change proposed and agree, if this proposal is approved, that we will provide all equipment, furnish the materials, except as may otherwise be noted above, and perform all services necessary for the work above specified, and will accept as full payment therefor the prices shown above.

NOTE: If you, the contractor, do not sign acceptance of this order, your attention is directed to the requirements of the specifications as to proceeding with the ordered work and filing a written protest within the time therein specified.

Contractor Acceptance by

Signature	(Print name and title)	Date
-----------	------------------------	------

CONTRACT CHANGE ORDER MEMORANDUM

DATE: 6/1/2007

Page 1 of 3

TO: PETER SIEGENTHALER / DOUG COE, Supervising Br. Eng.			FILE: E.A. 04 - 012024	
FROM: DOUG COE, Supervising Br. Eng.			CO-RTE-PM 04-SF,Ald-80-13.9/14.3,0.0/1.6	
			FED. NO. ACIM-080-1(085)8N	
CCO# 104	SUPPLEMENT#: 1	Category Cod CHSA	CONTINGENCY BALANCE (incl. this change) \$5,784,340.69	
COST: \$1,867,645.00			INCREASE <input checked="" type="checkbox"/> DECREASE <input type="checkbox"/>	
SUPPLEMENTAL FUNDS PROVIDED: \$0.00			HEADQUARTERS APPROVAL REQUIRED? <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO	
IS THIS REQUEST IN ACCORDANCE WITH ENVIRONMENTAL DOCUMENTS? <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO				
CCO DESCRIPTION: Additional Painting			PROJECT DESCRIPTION: REPLACE SFOBB EAST SPAN	
Original Contract Time: 1000 Day(s)	Time Adj. This Change: 0 Day(s)	Previously Approved CCO Time Adjustments: 424 Day(s)	Percentage Time Adjusted: (including this change) 42 %	Total # of Unreconciled Deferred Time CCO(s): (including this change) 0

THIS CHANGE ORDER PROVIDES FOR:

Painting of the following galvanized or otherwise coated steel components: 1) The traveler support rails on the bikepath and Orthotropic box girders, 2) The bikepath handrail, attachment pieces, light fixtures, and emergency gates except for the emergency gate wheels, 3) The PVC coated conduit on the bikepath, 4) The belvedere benches on the bikepath belvederes, 5) The service platform access stairs and railing, 6) The service platform handrails and C200 support channel face, 7) The cabinets, conduits, and support legs on the service platforms, except for any stainless steel components, rubber gaskets, nameplates, and sealflex conduit, 8) The overhead and CMS sign support structures, 9) The 6, 10, and 12 NPS ductile iron pipe, on the outside of the Orthotropic box girders, including the transition, fittings, supports and hangers, and 10) The substation access stairs and railings from the barrier rail to and inclusive of the first landing except for the bottom of the stairs and the bottom of the landing surface.

In mid-2006, an architectural review of the project and contract documents revealed that some visible steel surfaces, which are supposed to be painted white, are shown to be galvanized or otherwise coated per contract specifications and plans. This Change Order is needed to paint, with white paint the visible galvanized or otherwise coated steel components to meet the design intent.

The District 4, Bridge Architect, Clive Endress requires the additional areas to be painted as follows:

The galvanized and installed traveler scaffold support rails on the bikepath and orthotropic box girders will be painted over the galvanizing.

All of the galvanized bikepath handrailing elements, except for the emergency gate wheels, will be painted over the galvanizing.

The black PVC coated conduit which is fasten to the bottom of the bikepath panels will be painted with the finish coat directly over the PVC coating.

The belvedere benches designated to be shop painted with the finish coat will now be field painted after installation on the belvederes.

The access stairs, the handrails, and the outside face of the C200 support channel of the service platforms to be painted.

The components placed on the service platforms including cabinets, conduits, and supports except for any stainless steel components, rubber seals, nameplates, and sealflex conduits.

The specified to be galvanized overhead and CMS sign structures.

The ductile iron pipe specified to have a bituminous outside coating, will be replaced with ductile iron pipe with the specified zinc primer and painted with the specified finish coat.

The galvanized stairs providing access from the road deck to the sub-stations from the barrier rail to and including the first landing except for the bottom of the stair and landing surface.

All finish coats are to be applied after installation of the components to be painted.

Painting shall be done in accordance with the requirements specified in Section 10-1.53 "Clean and Paint Structural Steel" of the Special Provisions, as revised by Contract Change Order No. 104, Supplement 0, and the following revisions to the Special Provisions, of the SFOBB East Span Skyway Bridge Replacement Project, Bridge Nos. 340006L/R:

- 1.) Add the following sentence to Section 10-1.48, Traveler Support Rails, as paragraph six: "Traveler rails shall receive a finish paint coat in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."

- 2.) Add the following "Painting" sub-section to Section 10-1.51, Sign Structures: "Overhead sign structures shall be galvanized and painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."
- 3.) Revise the fifth paragraph of Sub-section "Steel pipe railings and hand railings" under Section 10-1.57, Miscellaneous Metal (Substation, Service Platforms, and Walkways), as follows: "Steel railings shall be galvanized and painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions. After galvanizing, all elements of the railing shall be free of fins, abrasions, rough or sharp edges, and other surface defects and shall not be kinked, twisted or bent."
- 4.) Add the following "Painting" sub-section to Section 10-1.57, Miscellaneous Metal (Substation, Service Platforms, and Walkways), after Sub-section, Steel pipe railings and hand railings; "Miscellaneous Metal in the Substations, Service Platforms, and Walkways designated to be painted shall be painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."
- 5.) Revise sub-section "Painting" of Section 10-1.58, Hinged Doors (Substations), as follows: "Except for the primer application specified herein, doors and frames shall be cleaned, prepared and painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."
- 6.) Add the following sentence to Section 10-1.62, Metal Bridge Railing, as paragraph two: "All galvanized bikepath railing elements shall be painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."
- 7.) Add the following sentence to Section 10-3.05, Conduit, Sub-section, Polyvinyl Chloride Coated Galvanized Rigid Steel Conduit, Threaded Couplings, and Elbows, as paragraph two: "The polyvinyl chloride (PVC) coated galvanized rigid steel (GRS) conduit, threaded couplings, and elbows designated to be painted shall be free from moisture, visible dust, visible grease, or other deleterious materials immediately prior to application of the finish coat in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."
- 8.) Revise paragraph five of Sub-section "Pipe and fittings for line 4 NPS or greater" of Section 10-4.04, CCSF Water Main and Sewage Force Main, as follows: "Ductile iron pipe and fittings shall have cement mortar lining and seal coating conforming to ANSI/AWWA C104/A21.4. Pipe shall have a bituminous outside coating conforming to ANSI/AWWA C151/A21.51 and fittings shall have a bituminous outside coating conforming to ANSI/AWWA C110/A21.10, except for the pipe and fittings on the outside of the orthotropic transition span, which shall be primed with "MC-FerroClad" TM or equal and painted in accordance with the requirements specified in Section 10-1.53, Clean and Paint Structural Steel, elsewhere in these special provisions."

Compensation will be Extra Work At Agreed Lump Sum of \$1,867,645.00. This agreed lump sum amount constitutes full compensation including all markups, for all direct and indirect costs, all associated project impacts including environmental and weather impacts, and all overhead costs. This additional project cost can be financed from the contingency funds provided for this contract. A detailed cost analysis is on file in the project records.

Mr. Pete Siegenthaler, SFOBB Construction Manager, Toll Bridge Construction, concurs with this change. A copy of the concurrence signed by Mr. Siegenthaler on February 14, 2007 is attached.

Prior Approval was obtained from Ms. Nancy Bobb, FHWA Bay Bridge Project Manager, via electronic message on April 4, 2007. A printed copy of the prior approval provided by Ms. Bobb is attached.

Prior Approval was obtained from Mr. Ken Terpstra, SFOBB Project Manager, via electronic message on April 6, 2007. A printed copy of the prior approval provided by Mr. Terpstra is attached.

Design concurrence was provided by Mr. Steve Hulsebus on August 8, 2006, as indicated by the attached E-mail.

Maintenance concurrence was obtained from Mr. Bill Zanetich, Structure Maintenance and Investigation, via electronic message on April 26, 2007. A printed copy of the prior approval provided by Mr. Zanetich is attached.

Authorization to proceed with this change order was issued by Headquarters on April 19, 2007.

CONTRACT CHANGE ORDER MEMORANDUM

EA: 012024 CCO: 104 - 1

DATE: 6/1/2007

Page 3 of 3

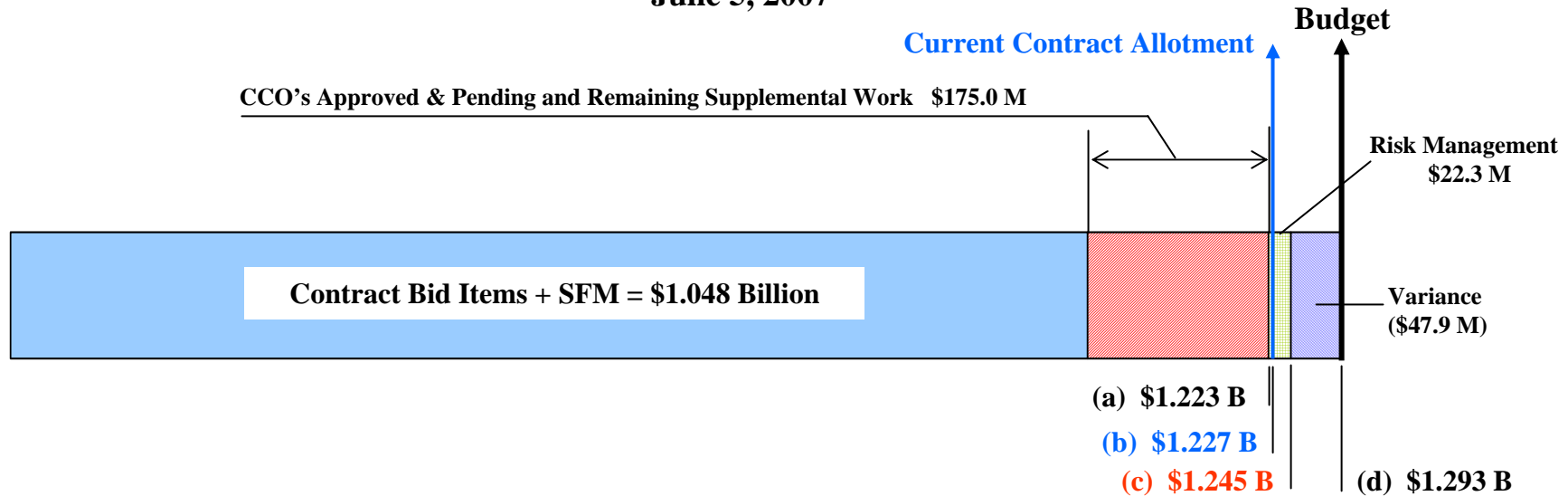
No adjustment of contract time is warranted for this change, as it does not affect the controlling operation.

CONCURRED BY:			ESTIMATE OF COST		
Construction Engineer:	Douglas Coe <i>DBC</i>	Date <i>6-1-07</i>		THIS REQUEST	TOTAL TO DATE
Bridge Engineer:	Douglas Coe <i>DBC</i>	Date <i>6-1-07</i>	ITEMS	\$0.00	\$0.00
Project Engineer:		Date	FORCE ACCOUNT	\$0.00	\$0.00
Project Manager:	Ken Terpstra	Date 4/6/07	AGREED PRICE	\$1,867,645.00	\$1,867,645.00
FHWA Rep.:	Nancy Bobb	Date 4/4/07	ADJUSTMENT	\$0.00	\$795,059.00
Environmental:		Date	TOTAL	\$1,867,645.00	\$2,662,704.00
Other (specify):	TBPOC	Date	FEDERAL PARTICIPATION		
Other (specify):	Andy Rogerson, Senior Chemical T	Date 4/19/07	<input checked="" type="checkbox"/> PARTICIPATING <input type="checkbox"/> PARTICIPATING IN PART <input type="checkbox"/> NONE <input type="checkbox"/> NON-PARTICIPATING (MAINTENANCE) <input type="checkbox"/> NON-PARTICIPATING		
District Prior Approval By:		Date	FEDERAL SEGREGATION (if more than one Funding Source or P.I.P. type)		
HQ (Issue Approve) By:	Ken Darby	Date 4/19/07	<input checked="" type="checkbox"/> CCO FUNDED PER CONTRACT <input type="checkbox"/> CCO FUNDED AS FOLLOWS		
Resident Engineer's Signature:		Date	FEDERAL FUNDING SOURCE PERCENT _____ _____ _____		

Skyway Contract 04-012024

Budget Analysis

June 5, 2007



Contract 04-012024 Skyway
Current Contract Budget Funding Status
 June 5, 2007 Basis

Contract Bid Items	\$	1,043,541,000
State Furnished Materials (SFM)	\$	4,276,439
Subtotal	\$	1,047,817,439
Supplemental Work	\$	6,565,700
Contingency At 5%	\$	52,616,861
Subtotal Original Contract Allotment	\$	1,107,000,000
Supplemental Budget Allocation Approved	\$	119,500,000
Subtotal Current Contract Allotment	\$	1,226,500,000 (b)
Remaining Unallotted Budget (Current Contract Budget - Current Contract Allotment)	\$	66,500,000
Total Current Contract Budget	\$	1,293,000,000 (d)

Reported Total Forecast At Completion \$1,293,000,000
 In 1st Quarter 2007 TBSRP Report

Contract 04-012024 Skyway
Contract Forecast At Completion (FAC) & Variance
 June 5, 2007 Basis

Contract Bid Items	\$	1,043,541,000
State Furnished Materials (SFM)	\$	4,276,439
Subtotal	\$	1,047,817,439
Supplemental Work Remaining	\$	6,026,577
CCO's		
CCO's (Approved (236) + Pending (22) = Total (258))	\$	167,082,287
CCO's = or > \$1Million Pending POC's approval (1)	\$	1,867,645
Subtotal	\$	1,222,793,948 (a)
Risk Management	\$	22,292,001
Total	\$	1,245,085,949 (c)

Variance (Total - Current Budget) \$ (47,914,051)

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California Department of Transportation

CONSTRUCTION DIVISION

TO: District 4 CCO Desk

Date: 4/19/2007

Contract No.: 4 - 012024

Road: SF, ALA-80-13.9/14.3, 0.0/1.6

FED. No.: ACIM-080-1(085)8N

To: SARTIPI - 04

Attention: 04 - SARWARY

HQ Direction:

TO PROCEED WITH THE WORK

CCO No. 104

Sup. No. 1

Rev. No. 1

Per Your Submittal Dated: 4/19/2007

CCO Category Code: C - H - S - A

PROVIDES FOR PAINTING GALVANIZED OR OTHERWISE COATED STEEL COMPONENTS IDENTIFIED WITHIN THE CCO.

RECOMMENDED AUTHORITY TO PROCEED IS CONDITIONAL ON THE FOLLOWING:

1. OBTAINING THE TBPOC'S AUTHORITY TO PROCEED WITH THIS CHANGE AS ITS COST EXCEEDS \$1,000,000.00.
2. THE COMPLETE AND TIMELY SUBMITTAL OF AN I&A REQUEST PACKAGE TO DIVISION OF CONSTRUCTION BY MAY 21, 2007.

THE TOTAL COST OF THIS CHANGE, INCLUDING THIS SUPPLEMENTAL, IS SHOWN AS \$3,295,059.00 WITH NO TIME ADJUSTMENT.

Items:	\$0.00
Force Account:	\$2,500,000.00
Agreed Price:	\$0.00
Adj. of Comp.	\$0.00

Total: \$2,500,000.00

Time: (NONE)

EUGENE MALLETT,
by: Assistant Division Chief

Ken Darby

Division of Construction
1120 "N" Street, MS-44, Sacramento, CA 95814
Fax Number: (916) 654-5735
To Confirm Transmission, Call (916) 654-5259

RECORD OF PRIOR APPROVAL FOR MAJOR CONTRACT CHANGE ORDERS

PROJECT NO. ACIM -080-1(085) 8N	CCO NO. 104 S1	CONTRACT NO. 04-012024	DIST-CO-RTE-PM 04-SF, Ala-80- 13.9/14.3, 0.0/1.6
REQUESTED BY: Jake VanEnckevort		CALTRANS HQ X RE/DISTRICT	DATE 4/3/07
<p>PROPOSED CHANGE:</p> <p>This change provides for painting of the following galvanized or otherwise coated steel components: 1) The traveler support rails on the bikepath and Orthotropic box girders, 2) The bikepath handrail, attachment pieces, light fixtures, and emergency gates except for the emergency gate wheels, 3) The PVC coated conduit on the bikepath, 4) The belvedere benches on the bikepath belvederes, 5) The service platforms access stairs and railing, 6) The service platforms handrails and C200 support channel face, 7) The cabinets, conduits, and support legs on the service platforms, except for any stainless steel components, rubber gaskets, nameplates, and sealflex conduit, 8) The overhead and CMS sign support structures, 9) The 6, 10, and 12 NPS ductile iron pipe, on the outside of the Orthotropic box girders, including the transition, fittings, supports and hangers, and 10) The substation access stairs and railings from the barrier rail to and inclusive of the first landing except for the bottom of the stairs and the bottom of the landing surface.</p>			
<p>REASON FOR CHANGE:</p> <p>This change is being processed because the Architect's intentions were not reflected on the contract plans.</p> <p>More information is attached in an email from Mr. Kannu Balan.</p> <p>AS DISCUSSED IN FIELD (REPORT NO. ____)</p>			
TIME EXTENSIONS _NONE _0_ DAYS ___ DEFERRED		ACTIVITY ON CRITICAL PATH AFFECTED BY CCO (IF TIME EXTENSION INVOLVED)	
ESTIMATE OF COST: \$ 2,000,000 INCREASE \$ ___ DECREASE		CCO: CONTRACT ITEM X AGREED PRICE	METHOD OF PAYMENT ADJUSTMENT OF COMPENSATION EXTRA WORK AT FORCE ACCOUNT
THE WORK COVERED BY THE PROPOSED REVISION AS DESCRIBED ABOVE IS APPROVED SUBJECT TO SUBMISSION OF SUPPORTING DOCUMENTATION INCLUDING COST EVALUATION AND JUSTIFICATION OF TIME EXTENSIONS.			
OTHER CONDITIONS:			
PRIOR APPROVAL TO PROCEED GRANTED BY: Nancy E. Bobb			DATE OF AUTHORIZATION: 4/4/07

**Item 2: San Francisco-Oakland Bay Bridge
e. Schedule and Budget Revisions
for Stormwater Treatment
Measures**

TO: Toll Bridge Program Oversight Committee (TBPOC) DATE: June 21, 2007

FR: Tony Anziano, Toll Bridge Program Manager, Caltrans

RE: Agenda No. - 2 e

Item- Stormwater Contract
Schedule and Budget Revisions for Stormwater Treatment Measures

Recommendation:

The PMT recommends that the TBPOC approve a revision to the current approved and forecasted contract budget and completion dates for the Stormwater Treatment Measures contract to \$18.3 million and March 2008, respectively.

Cost Impact:

There is no net impact to East Span Replacement Project Budget or to the overall Seismic Retrofit Program. The proposed \$3.3 million increase for the Stormwater Treatment Measures contract will be funded from available funding in the Other Budget Capital line item.

Schedule Impact:

There is no impact to the overall completion or open-to-traffic dates of the East Span Replacement Project Schedule. The proposed change will adjust the forecast June 2007 contract completion date to match the current approved contract completion date of March 2008.

Discussion:

The Stormwater Treatment Measures contract (04-0120J4) was awarded on March 20, 2006 to Diablo Contractors with a current approved budget of \$15 million. The contract installs a number of best practice stormwater treatment measures from the Macarthur Maze to the Oakland Touchdown. The work includes installation of new retention basins, piping, and other drainage infrastructure. Based on an accelerated A+B bid, the contractor submitted a bid to complete the work by June 2007, which is 9 months earlier than the current approved contract completion date of March 2008.

During construction, a number of issues have arisen, including the Maze collapse, nesting birds, unknown buried man-made objects, unidentified utilities, and the discovery of unsuitable materials, that have resulted in a number of approved, pending, and future potential CCO's and risks. Based on the current Risk Management assessment (see attached) of the contract, identified risks require a contract budget increase of \$3.3 million to complete the contract (see attached "Budget Balance Beam") and a contract completion schedule extension.

To cover the projected contract forecast, the PMT recommends that the TBPOC transfer \$3.3 million from available funding in the Other Budgeted Capital line item of the East Span Replacement Project Budget to the Stormwater Treatment Measures Contract and request BATA to allocate the additional funds as soon as possible. The transfer will result in no net change to the East Span Replacement Project Budget. The budget changes will be reported in the next monthly progress report as per the summary table below:

Summary of Proposed Budget Table for June 2007 Monthly Report

SFOBB East Span Replacement Project Budget	AB144/SB66 Baseline Budget (07/2005)	Current Approved Budget (05/2007)	Forecast Budget (05/2007)	Variance from Current Approved
Stormwater Contract	15.0	18.3	18.3	–
Other Budgeted Capital	35.1	31.8	7.7	(24.1)
Subtotal of Other COS and CO	5,436.5	5,615.7	5,648.7	33.0
Total	5,486.6	5,665.8	5,674.7	8.9

**** Proposed Modifications to Report Table***

Construction delays have impacted the current contract schedule by at least three months. Nesting birds, other environmental and differing site condition delays may further postpone the completed date of the contract. These delays will not impact the overall completion or open to traffic dates of the East Span Replacement. The PMT recommends that the TBPOC revise the forecasted contract completion date for the contract back to March 2008 until all environmental issues are completely resolved.

The revised date matches the AB144/SB66 and current approved contract completion date. See table below:

Summary of Proposed Schedule Table for June 2007 Monthly Report

Stormwater Treatment Measures Contract (04-0120J4)	AB144/SB66 Baseline Contract Complete Schedule	Current Approved Contract Complete Schedule	Forecast Contract Complete Schedule	Schedule Variance from Approved
Current	March 2008	March 2008	June 2007	(9)
Proposed Revised	March 2008	March 2008	March 2008	-

**** Proposed Modifications to Report Table***

Attachments:

- 1) Stormwater Quantitative Analysis as of June 5, 2007
- 2) Stormwater Treatment Measures Contract 04-0120J4, Budget Analysis, June 5, 2007



Stormwater - Quantitative Analysis

as of June 5, 2007

ID #	Threat Opportunity Event	Cost Range			Probability		Probable Cost
		Most Likely	Low	High	Low	High	
Risks							
8	Water intrusion into excavations greater than anticipated by the contract causes delay and/or increased costs to occur.		\$100,000	\$200,000	80%	100%	\$135,000
18	Increased cost due to environmental impacts.	\$160,000	\$0	\$200,000	80%	100%	\$108,000
41	Pipe Joint Repairs		\$50,000	\$200,000	60%	80%	\$87,500
19	Construction Delay (TRO+)	\$120,000	\$60,000	\$180,000	60%	80%	\$84,000
10	A buried manmade object causes delay and/or increased costs to occur. Does not include utilities or item #1 (Footings at Location 5).	\$100,000	\$25,000	\$150,000	80%	100%	\$82,500
11	A differing site condition causes delay and/or increased costs to occur. Does not include water intrusion, utilities or buried manmade objects.	\$70,000	\$60,000	\$90,000	80%	100%	\$66,000
13	Construction impacts public traffic more than provided for in the contract.	\$0	\$0	\$50,000	20%	40%	\$5,000
16	Delays from adjacent (non SFOBB) contracts may cause delay to this contract.	\$0	\$0	\$50,000	0%	20%	\$2,000
17	Department not reviewing submittals in a timely manner causes a delay to the contract.		\$0	\$20,000	0%	20%	\$1,000
23	Weather day delay	\$0	\$0	\$0	80%	100%	\$0
10 Risks - Total Risk Cost:							\$571,000
CCOs							
36	Project Impacted by MacArthur Maze Bridge Collapse (Excludes TRO)		\$0	\$1,000,000	100%	100%	\$500,000
28	Import Borrow Overrun at Area 5	\$350,000	\$250,000	\$500,000	100%	100%	\$367,000
37	Known Delays Due to various CCOs expected through completion of Oct. 2007.	\$360,000	\$180,000	\$540,000	100%	100%	\$360,000
33	Restore Highway Lighting Near Drainage System Installations		\$100,000	\$300,000	100%	100%	\$200,000
34	Planting Mitigation for BCDC	\$200,000	\$100,000	\$300,000	100%	100%	\$200,000
38	Additional Class II Disposal		\$100,000	\$200,000	100%	100%	\$150,000
29	K Rail Overrun	\$150,000	\$100,000	\$200,000	100%	100%	\$150,000
25	Miscellaneous Future CCOs	\$138,938	\$0	\$277,877	100%	100%	\$139,000
35	Restore Irrigation Near Drainage System Installations	\$50,000	\$20,000	\$100,000	100%	100%	\$57,000



Stormwater - Quantitative Analysis

as of June 5, 2007

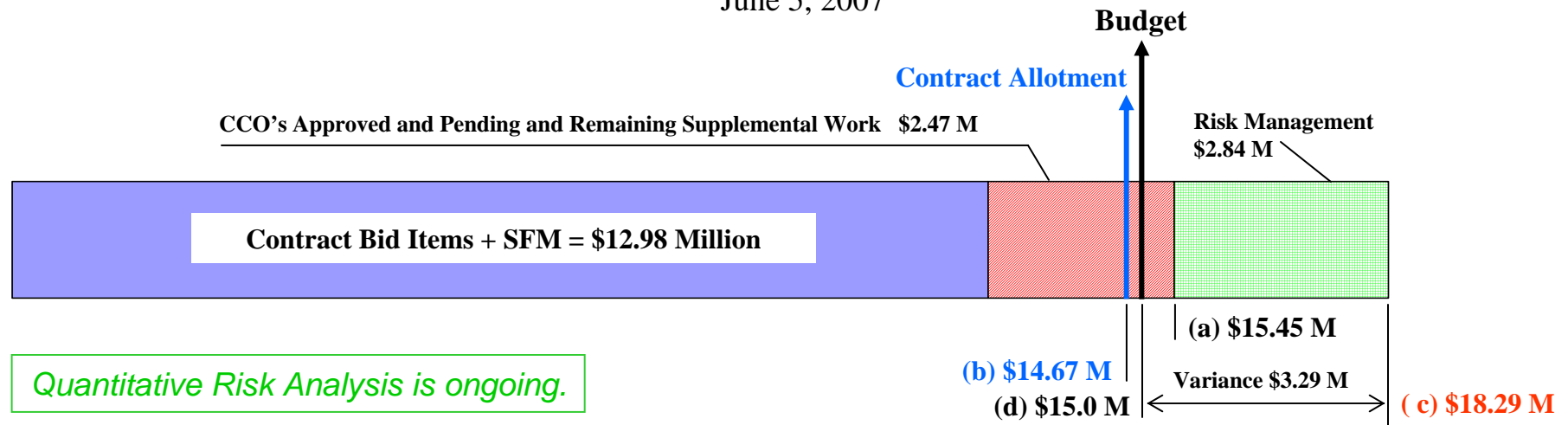
ID #	Threat Opportunity Event	Cost Range			Probability		Probable Cost
		Most Likely	Low	High	Low	High	
40	AC Removal at WB80 RCP Crossing	\$50,000	\$0	\$100,000	100%	100%	\$50,000
30	Minor Concrete Overrun	\$50,000	\$0	\$100,000	100%	100%	\$50,000
1	A footing at location 5 causes additional concrete removal (differing site condition)	\$50,000	\$20,000	\$60,000	100%	100%	\$43,000
2	Dewatering and hazardous waste generation fees add cost to the project.		\$1,600	\$12,100	100%	100%	\$7,000
13 CCOs - Total CCO Cost:							\$2,273,000

Subtotals	Risks:	\$571,000
	NOPCs:	\$0
	CCOs:	\$2,273,000
Risk Management Cost		\$2,844,000

Stormwater Treatment Measures Contract 04-0120J4

Budget Analysis

June 5, 2007



Contract 04-0120J4 Stormwater Treatment Measures Current Contract Budget Funding Status

June 5, 2007 Basis

Contract Bid Items	\$	12,864,663	
State Furnished Materials (SFM)	\$	112,100	
Subtotal	\$	12,976,763	
Supplemental Work	\$	368,000	
Contingency At 10%	\$	1,325,237	
Subtotal Original Contract Allotment	\$	14,670,000	
Supplemental Budget Allocation Approved	\$	-	
Subtotal Current Contract Allotment	\$	14,670,000	(b)
Remaining Unallotted Budget (Current Contract Budget - Current Contract Allotment)	\$	330,000	
Total Current Contract Budget	\$	15,000,000	(d)

Reported Total Forecast At Completion \$ 15,000,000
In 1st Quarter 2007 TBSRP Report

Contract 04-0120J4 Stormwater Treatment Measures Contract Forecast At Completion & Variance

June 5, 2007 Basis

Contract Bid Items	\$	12,864,663	
State Furnished Materials (SFM)	\$	112,100	
Subtotal	\$	12,976,763	
Supplemental Work	\$	254,500	
CCO's			
CCO's (Approved (31) + Pending (10) = Total (41)	\$	2,215,618	
CCO's = or > \$1 Million Pending POC's approval on DATE	\$	-	
Subtotal	\$	15,446,881	(a)
Risk Management	\$	2,844,000	
Total	\$	18,290,881	(c)

Variance (Total - Current Budget) \$ 3,290,881

Confidential Draft – For Deliberative Purpose Only

Item 3: New Benicia-Martinez Bridge

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** June 21, 2007

FR: Tony Anziano, Caltrans Toll Bridge Program Manager
Peter Lee, BATA Senior Transportation Engineer

RE: Agenda No. - 3

ITEM: New Benicia-Martinez Bridge Project – Contract Approval
Construction of Modifications to Existing Bridge (EA 04-0060A4)

Recommendation:

Refer to BATA and CTC for approval and funding a contract to construct modifications to the existing Benicia-Martinez Bridge (EA 04-0060A4) as part of the Regional Measure 1 New Benicia-Martinez Bridge Project.

Schedule Status:

The contract will be ready-to-list on July 2, 2007. Pending BATA approval of bid documents and RM1 funding and CTC approval of SHOPP funding on July 25, 2007, the contract is scheduled for advertisement on July 30, 2007. Bid opening is scheduled for September 18, 2007 with award soon after. The current estimated duration of the contract is 2 years and 3 months, which forecasts the work to be completed by the end of 2009 – six months earlier than currently reported in the monthly report.

Cost Status:

The current forecast for the contract is within the overall New Benicia-Martinez Bridge project budget. (See Budget and Funding Analysis Table) The current forecast contract budget is \$61 million – \$50 million RM1 and \$11 million SHOPP. The additional SHOPP work scope will result in project budget change of \$11 million.

Discussion:

The Department is currently completing the PS&E package to convert the existing Benicia-Martinez Bridge to carry southbound only traffic and to add a bicycle/pedestrian pathway. The contract has a number of activities, including:

- Partial reconstructing of the existing bridge deck (Rehabilitation)

- Removing the existing median barriers to allow increase of the number of southbound Interstate 680 lanes from three to four with shoulders (RM1)
- Constructing a new barrier on the west side of the bridge to provide for a new pedestrian/bicycle lane as part the Bay Trail network (RM1)
- Reconstructing the alignment of portions of the Interstate 680/Marina Vista Interchange for the new lane configuration and to remove undulations due to poor soil conditions (RM1 and SHOPP)
- Demolishing of the decommissioned toll plaza in Benicia (RM1)
- Regrading of area north of bridge (RM1)

The PMT had raised earlier concerns on the constructability of the deck rehabilitation work due to the unbalanced loading of the bridge after northbound traffic is moved to the new bridge. Department staff, including Brian Maroney (Deputy Program Manager) and Ken Brown (Office of Structures Investigation and Maintenance), has inspected the bridge and have determined that the proposed reconstruction strategy is feasible and prudent for the existing bridge with some contract changes. The contract changes, to be incorporated into the project plans, will include maintaining the existing load restrictions on the exterior lanes of the bridge. Currently, trucks are not allowed in the extended cantilevered exterior lanes of the bridge.

The current forecast contract budget for the project is \$61 million – \$50 million RM1 and \$11 million SHOPP. The \$50 million in RM1 funding includes \$16 million in available project contingency that the TBPOC has approved for use for the deck rehabilitation work. Both BATA and CTC have been requested to take their respective funding actions for the contract in July 2007. The additional SHOPP work scope will result in budget change of \$11 million. Including \$22.5 million for current risk management estimates, there still remains approximately \$4.5 million in unencumbered project contingency.

Per TBPOC's request, the Department has accelerated Office Engineer's review of the project bid documents by nearly two months. (See attached schedule comparison) Pending the bid document and funding approvals, the Department is now prepared to advertise the contract on July 30, 2007. Bid opening is scheduled for September 18, 2007 with award soon after. The current estimated duration of the contract is 2 years and 3 months, which forecasts the work to be completed by the end of 2009. This completion date would be six months earlier than currently reported in the monthly report. The contract will be advertised as an A+B contract, so the contract duration maybe further reduced when bids are opened. Staff proposes to update the reported completion schedule after the contract is awarded.

Staff has reviewed the PS&E package for the contract and requests that the TBPOC approve the package for BATA bid document approval and BATA/CTC funding approval with the Department's recommended lane restrictions.

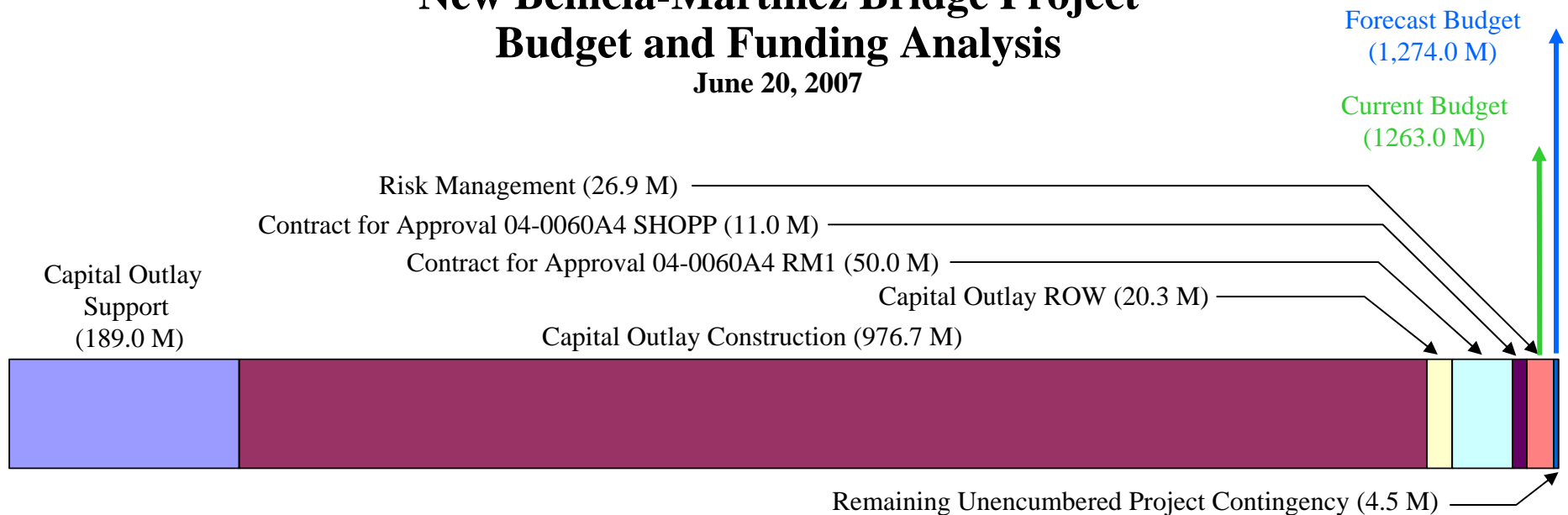
Attachments:

- ◆ New Benicia-Martinez Bridge Project Budget and Funding Analysis, June 20, 2007
- ◆ Contract Award Schedule Comparison

New Benicia-Martinez Bridge Project

Budget and Funding Analysis

June 20, 2007

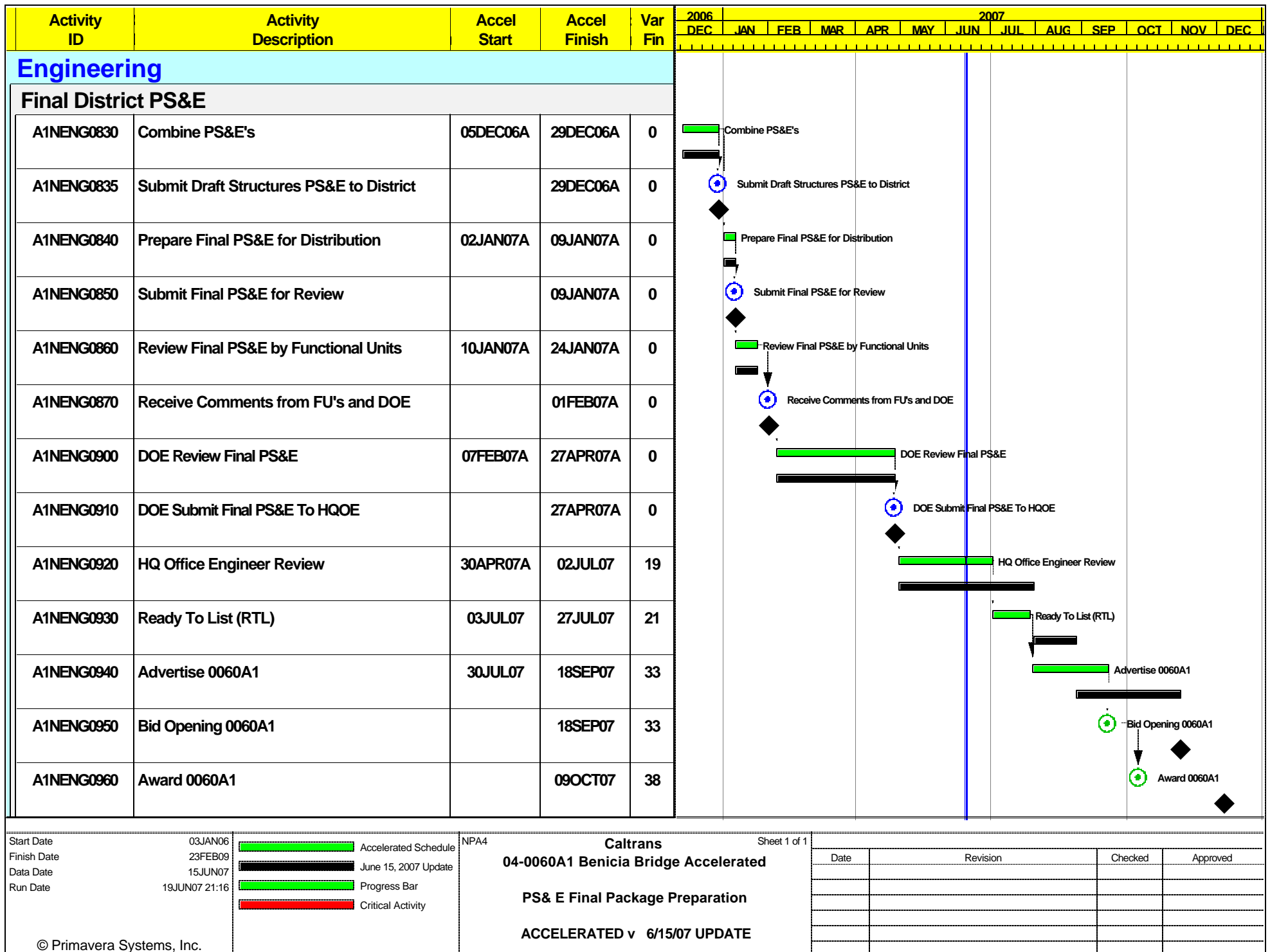


New Benicia-Martinez Bridge Project
Current Forecast Project Budget
 June 20, 2007 Basis

Capital Outlay Support	\$	189.0
Capital Outlay Construction (Excluding Contract for Approval)	\$	976.7
Capital Outlay Right-of Way	\$	20.3
Subtotal	\$	1,186.0
Contract For Approval		
Existing Benicia Bridge Modifications (04-0060A4) (RM1)	\$	50.0
Existing Benicia Bridge Modifications (04-0060A4) (SHOPP)	\$	11.0
Subtotal	\$	61.0
Risk Management		
Capital Outlay Support	\$	5.0
Capital Outlay Construction	\$	17.5
Total Risk Management	\$	22.5
Total Project Forecast	\$	1,269.5

New Benicia-Martinez Bridge Project
Current Forecast Project Funding
 June 20, 2007 Basis

Current Budget		
Regional Measure 1 Funds	\$	1,224.7
Non-Toll Funding (including Prop. 192 and Prior SHOPP)	\$	38.3
Total Current Budget	\$	1,263.0
Funding for Approval		
Existing Benicia Bridge Modifications (04-0060A4) (SHOPP)	\$	11.0
Total Forecast Budget	\$	1,274.0
Total Project Forecast	\$	1,269.5
Forecast Unencumbered Project Contingency	\$	4.5



Item 4: Other Business

No Attachments